

■ Astoria Waterfront ■

P l a n n i n g S t u d y



■
Astoria, Oregon



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■ Astoria Waterfront ■

P l a n n i n g S t u d y

Prepared for the
City of Astoria

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■
MAKERS

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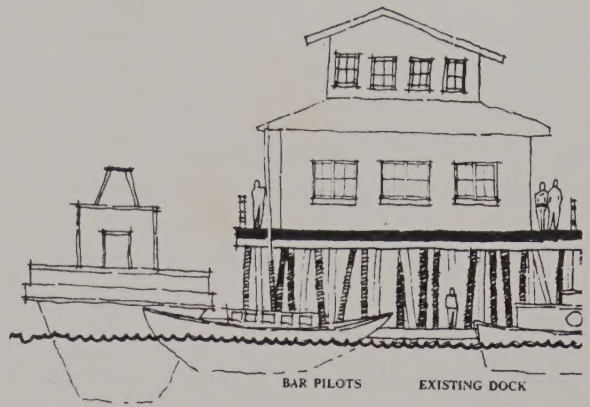
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INTRODUCTION

INTRODUCTION

PROJECT DESCRIPTION

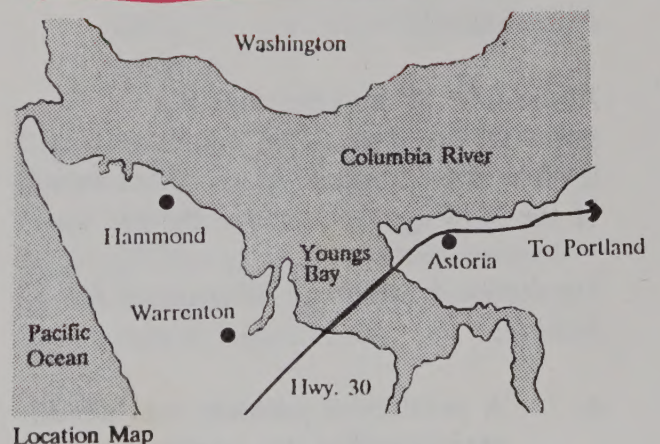
The Astoria waterfront, once a bustling area occupied by fish processing and lumber industries, has declined in recent years. Though it is unlikely these industries will ever become what they once were, redevelopment of the waterfront is necessary for the City's future viability. Identifying and developing the direction of this change is the essence of this urban design study.

In 1986, the City completed a comprehensive planning study for the revitalization of the Columbia River waterfront. The plan focused on the redevelopment of the historic area between 6th and 20th Street. The plan's objective was to create a mixed-use, water-oriented waterfront that would function as a tourist destination, and the plan served as a catalyst for the implementation of the Sixth Street River Park.

This plan provided incentive to City officials and residents that the waterfront could be revitalized without altering the vitality and historic feeling that presently exist within the area. It was determined that a master plan which was sensitive to the aesthetics of the area and the goals and desires of the community should be the focus of any new development.

To accomplish this goal, three committees, a Citizen Advisory Committee (CAC), a Technical Advisory Committee (TAC), and representatives from affected property owners (APO) were formed along with the selection of the Murase Associates/Makers design team to develop a plan for the downtown portion of the riverfront. The project comprised a 14-square block area bounded by the Columbia River to the north, the Columbia River Maritime Museum to the east, the Sixth Street River Park to the west, and Marine Drive to the south.

From the onset of this project, it was the goal of the committee members and design team to build and expand upon the indigenous qualities found within this 14-block area. This was accomplished by the development of a long-range master plan that provided the frame-work for a waterfront area where residents and tourists alike could enjoy the open sky, magnificent views of the river and the life upon it.



PROJECT SUMMARY

The Master Plan is a result of the combined efforts of public officials, affected property owners, the citizen's advisory committee, the technical advisory committee and various concerned citizens. It was through their invaluable help and guidance that the design team was able to develop the master plan for the Astoria downtown riverfront area.

A central factor in this study was whether and to what extent the Burlington Northern right-of-way would be available for public access. The 50 foot right-of-way bisects the project area and has the potential to be the spine of pedestrian circulation through the waterfront. As a pedestrian path the right-of-way could provide the needed linkage between the Columbia River Maritime Museum and the Sixth Street River Park.

Due to the complexities and uncertainties involved with public access along the right-of-way, two alternatives of the master plan concept have been developed: Alternative A and Alternative B.

Alternative A demonstrates the potential the waterfront has if the City is able to gain access to a portion or all of the Burlington Northern right-of-way.

Distinctive features of Alternative A include:

- o A pedestrian pathway that would extend within the northern

portion of the right-of-way from 6th to 15th Street. The pathway would then continue on private property adjacent to the northern side of the right-of-way, along the bankline of the river, east to the Columbia River Maritime Museum. Pathway improvements would include lighting, street furniture and historical and informational kiosks.

- o Eventual implementation of a Trolley or rubber-tired vehicle system from the Sixth Street River Park to the Columbia River Maritime Museum. The Trolley could stop at designated locations through the downtown riverfront area and possibly expand from Smith Point, at the far west end of the City, east to Tongue Point.

Alternative B demonstrates how a waterfront plan could be developed without access to the Burlington Northern right-of-way.

Distinctive features of Alternative B include:

- o A pedestrian pathway that intersects but does not follow the right-of-way from 6th to 15th Street. As in Plan A, the path would continue along the northern side of the right-of-way, along the bankline of the river, east to the Columbia River Maritime Museum.

Key features common to Alternative A and Alternative B include:

- o Improved and increased off-street parking between 5th and 8th Streets, Pier 11, the Brix Maritime Corporation Building, and the area immediately west of the Museum.
- o A redeveloped streetscape, that would include sidewalk resurfacing, lighting, street furniture, and street trees within the Astor Street area and along 8th, 10th, 11th, 12th, 14th and 17th Streets.
- o ✓ Expanded deck and view area at the foot of 14th Street.
- o Expansion of sidewalk, south of the Burlington Northern Railroad right-of-way, between 14th and 15th Streets with improvement of the landscape buffer.
- o Redevelopment of the pile supported Brix Maritime Corporation Building, located along the bankline between 12th and 14th Streets, to accommodate increased parking and special weekend events such as a farmer's market, or an arts and craft fair.
- o ✓ Expanded deck and viewing area east and west of Pier 11.
- o A multi-level wood structure river access area with a shelter and

sculptural focal point between 11th and 12th Streets.

- o A small summer day moorage area located at the foot of 12th Street.
- o A hard-surfaced concrete, multi-leveled area immediately west of the Maritime Museum to provide river access for residents and visitors.
- o Enhancements to the Maritime Museum Plaza which could include a tower, large ship and interpretive displays, information center and plantings.
- o A small amphitheater and new pedestrian plaza, east of the Maritime Museum.
- o Redevelopment of the existing train station as a restaurant or tourist-oriented commercial use.
- o A seasonal moorage facility located east of the Maritime Museum to satisfy the need for a downtown public marina and allow for day visitors to the museum.

The preparation of this Master Plan was funded in part with a grant from the Oregon State Lottery through the Regional Strategies Fund administered by the State of Oregon Economic Development Department.

WATERFRONT OBJECTIVES

The City of Astoria's goals for the waterfront are as follows:

- o Enhance and promote re-development within the historic waterfront while protecting existing water-related business and the present "working waterfront character".
- o Develop the waterfront as a source of enjoyment and pride for Astoria residents. Make it a place so inviting that residents and tourists alike will stroll along the river and through the historic working waterfront with its new interpretive viewing areas and other visitor amenities.

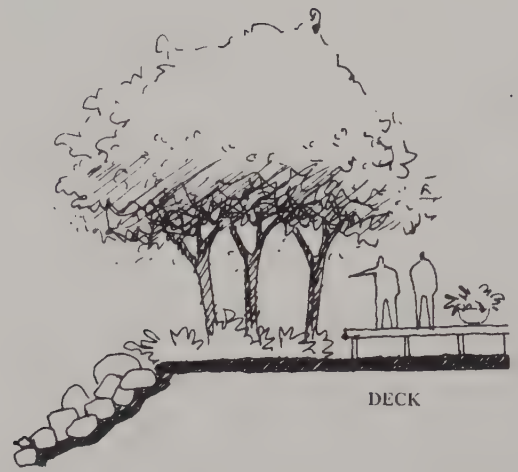
The objectives listed below are based on meetings with city officials, the citizen and technical advisory groups, affected property owners and concerned citizens.

- o Maintain the "existing working waterfront" feeling of the downtown river's edge. Existing uses will be provided for while new uses are promoted.
- o Create a magnet for drawing people to the central business core and other points of interest within the city.
- o Develop a pedestrian circulation path which will link the Sixth Street River Park with the

Columbia River Maritime Museum.

- o Provide more accessible and higher quality view areas of the river and the activity upon it.
- o Develop areas that will allow visitors to walk down to the river.
- o Increase accessibility from the water to the land through development of moorage areas within the waterfront area.
- o Improve linkage between the riverfront and the central business district and other points of interest to promote more intensive use of the waterfront and enhance the economic vitality of the downtown area.
- o Provide public spaces on the waterfront which permit a wide range of uses rather than single-purpose uses.
- o Respect and build upon the historic character of the waterfront through identification and preservation of historic landmarks and buildings of historic importance or architectural interest.
- o Minimize conflict between existing and proposed uses on the waterfront.

- o Improve general appearance of the area through development of entry points, under-utilized spaces, and through buffering of undesirable views such as parking.
- o Provide more parking throughout the area. Improve vehicular circulation and minimize potential truck/car conflict between business and tourist-related traffic.
- o Provide for handicapped access.
- o Develop an unimpeded access route for security vehicles.



HISTORY

HISTORY

As the gateway to the Columbia River, Astoria possesses a rich and complex history. Spanish explorers first claimed the territory of the Pacific from Tierra del Fuego to Alaska for the Spanish crown in the mid-sixteenth century, but the first recorded sighting of the Columbia River was not until 1775. The Lewis and Clark overland expedition reached the Pacific and established Fort Clatsop in 1805, located 7 miles southwest of Astoria. The fort was abandoned in 1806.

John Jacob Astor commissioned a fur-trading post in Astoria 1811, and the first white settlement west of the Rockies was started. The settlement was home to approximately 30 inhabitants until the 1840's when new settlers began to join the existing settlement. Astoria was incorporated in 1856, and growth was slow for the next 20 years.

During this period, the downtown experienced limited commercial development in the form of canneries, sawmills, wharfs, warehouses, machine shops, stores and hotels. Due to the city's topography, growth was restricted to flat areas at the base of surrounding hills and on timbered pilings over the shoreline.

By 1891 there were 22 canning facilities, and Astoria was home to a thriving salmon packing industry and a slower

growing saw and planing mill industry. Immigrant influxes began at this time with the Chinese being the first arrivals.

The Chinese business and dwelling district was a 3x4 block area bounded by Astor and Commercial Streets, and 6th and 10th Streets. By 1910, there were almost 400 Chinese residents. Scandinavian and Finnish were the next groups to locate within the city.

The first of two major downtown fires occurred in 1883 on Commercial Street between 14th and 17th Streets. Damage was estimated at 2 million dollars. Due to the economic viability of the area, the downtown was rebuilt within a short period of time.

During the 1880's, Astoria was the primary entertainment spot on the route between San Francisco and Seattle. "Swilltown", as the area was to be known, was famous for its gambling saloons and red light district.



1922 Astoria Fire

Growth continued until shortly after the turn of the century when a decline in the salmon industry weakened Astoria's economy. A second major fire occurred in 1922 and changed the face of downtown Astoria. Over 32 city blocks, approximately 40 acres, were destroyed. Damage was extensive since almost all buildings were constructed of wood. Wooden planked streets were supported on wooden viaducts, and this system allowed the fire to pass through underground air tunnels and ignite buildings blocks away. In total, 33 buildings were destroyed and property damage estimates ranged from 12 to 20 million dollars.

The reconstruction of downtown consisted of 2 major phases: one, public and the other, private. The first phase of public work consisted of widening and rebuilding the destroyed street system. Pilings were located along curb lines and concrete walls built on these. The walls were built in what is known as a chair-type construction method. The spaces between the walls were filled with sand dredged from the Columbia River and concrete pavement was then placed over the fill. Sidewalks, street lighting, and signage was replaced at this time as well.

The majority of reconstructed buildings were of either brick or concrete. Most commercial buildings were situated between Marine Drive to the north and Exchange Street to the south. Buildings between 8th Street to the west and 15th Street to the east were constructed on concrete piers over former tidelands.

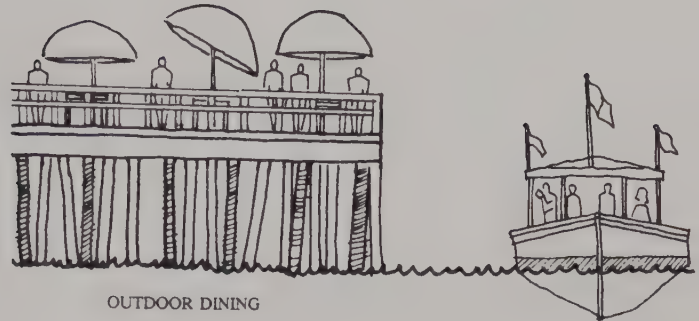
None of the former wooden plank streets remained.

The architecture of the reconstruction period was generally streamlined in form and ornamentation. All facades were flat and predominately one-story.

North of Water Street between 5th and 17th Streets the area was rebuilt specifically for waterfront uses such as canneries, net drying or storage facilities. Structures were unattached and connected only by adjoining piers. Buildings were generally constructed on the edge of the shoreline. They were built on pilings and used a wood truss support system. Shed and gable roofs were both common throughout the area. Building walls were generally fabricated of horizontal wooden channel board or vertical board and batten.



Plank Street Example



OUTDOOR DINING

EXISTING CONDITIONS

EXISTING CONDITIONS

SETTING

Today, the historic nature of the city, the Columbia River, the working waterfront, the Columbia River Maritime Museum, historic homes, the downtown area and a variety of cultural centers are attributes that give Astoria its vitality.

As the gateway to the Pacific Ocean, the river is an important commercial channel and is widely used by recreational boaters. River activity is diversified and active, making river watching an important and enjoyed activity. River views are expansive but the public only has access to them at the Sixth Street River Park and the Maritime Museum Plaza. In addition, there is no place to gain physical access to the water anywhere within the project area.

Pleasure boat moorage exists east and west of the site at the West Mooring Basin adjacent to the Red Lion Inn and at the East Mooring Basin, approximately 17 blocks to the east. No day moorage space is presently available in the downtown area for recreational boaters.

The existing working waterfront character is a direct result of the water dependent/related uses found within the area. Present waterfront activities are of special interest to residents and tourists alike, and include viewing the unloading of fishing boats, fish

processing businesses, tug boat activities, and the large ships that travel the channel in close proximity to the shoreline.

There is no distinct pedestrian circulation system along the shoreline at this time. Individuals who walk through the area generally use the railroad right-of-way.

Train travel through the downtown area presently averages 1 trip per week and usually consists of an engine and one or two box cars.



Seafood Processing Facility

The Columbia River Maritime Museum is considered to be one of the best of its type in the nation. Over 100,000 visitors explore the facility each year, and this is expected to continue. After experiencing the museum, tourists may have lunch, shop, or visit

other attractions and then leave the city to explore the oceanfront to the south.

Pedestrian connections to the downtown core are in need of visual strengthening and pedestrian walkways across the heavily used Marine Drive need to be developed.

Parking, a major concern of many waterfront property owners, is felt to be inadequate within the waterfront area. Though there is off-street and on-street parking space available, it is insufficient to meet present or future demand.

The Columbia River Maritime Museum and the Sixth Street River Park act as east and west anchors to the downtown riverfront. Even with these attributes there is a lack of arrival to or departure from the waterfront area for visitors.



Burlington Northern Railroad Right-of-Way

LOCATION

Astoria is situated on a peninsula in the northwest corner of Oregon, within Clatsop County. The peninsula is defined by Youngs Bay to the south and the Columbia River to the north. The Columbia River forms the boundary between Oregon and Washington.

LAND USE

The study area contains a mixture of water-dependent/related and nonwater-dependent/related uses, many of which extend over the water on pile supported structures. Major land uses include:

5th to 7th Street	Retail, office, service stations and commercial.
7th to 8th Street	Fish processing, storage and office.
8th to 9th Street	Fish processing, storage, office and commercial.
9th to 10th Street	Office, storage, commercial and public utility.
10th to 11th Street	Restaurant and commercial.
11th to 12th Street	Restaurant, commercial, movie theater, and storage.
12th to 14th Street	Bar Pilot Office, storage and commercial.
14th to 15th Street	Office, radio station and marine supply
15th to 16th Street	Office

16th to 17th Street Vacant and
Columbia River
Maritime Museum.

Many of the buildings within the area are vacant or underutilized and are oriented to Marine Drive and Astor Street and not the river.

VIEWS

Viewing the river and associated waterfront activities is an important component of the visitor experience. Major views include the river itself, the large ships that are easily observed due to the closeness of the main navigational channel and waterfront activities which include: shipping, seafood processing, tug operations, and the bar/river pilots.

Vehicles parked along the railroad right-of-way and existing evergreen trees between 16th and 17th Streets presently block river views.

Vistas down streets to the river, although blocked in several locations, offer magnificent views to river activity and a dramatic backdrop of the Washington shore.

Views to the shoreline from the river are impressive and include the downtown district and the adjacent hillside residential community.

Important and potential view areas within the downtown riverfront include:

● Sixth Street Riverfront Park,

- 7th Street terminus, presently blocked with seafood processing related storage,
- 8th Street terminus,
- Bornsteins and Ocean Foods,
- North side of railroad right-of-way between 9th and 10th Streets,
- Existing over-water decks east and west of Pier 11,
- North side of railroad right-of-way between 11th and 12th Streets,
- 12th Street terminus,
- 14th Street terminus,
- North side of railroad right-of-way from foot of 15th Street east to the Maritime Museum, and
- Maritime Museum east to the former railroad station.



Columbia River Looking East

PEDESTRIAN CIRCULATION

There is no designated east/west pedestrian circulation system through the waterfront area. Pedestrians presently walk along the Burlington Northern right-of-way between 6th and 14th Streets, but, because of spacing between the wood planks and other slippery conditions, the path is difficult and unsafe.

Marine Drive is a deterrent to pedestrian circulation between the downtown area and the waterfront district due to the amount of traffic it receives and unclear pedestrian crossing.

VEHICULAR CIRCULATION AND PARKING

Vehicular circulation, north of Marine Drive, is directly related to the mixture of water-dependent/related and nonwater-dependent/related uses found within the project area.

Periodically, between 7th and 9th Streets, congestion occurs due to the loading and unloading of materials related to the seafood processing facilities located there.

Space for trucks and other large vehicles is needed for water-dependent uses found at the foot of 11th, 12th, and 15th Streets. Trucks currently park within the BNR right-of-way east of 11th Street.

There is a demand by all waterfront businesses for more parking. Existing

parking areas within the waterfront area offer the potential of being redeveloped and expanded.

COLUMBIA RIVER

The Columbia River at Astoria is approximately 4 miles across, is fresh water, and has a tidal variation (difference between highest high and lowest low) of 11 feet.

The main navigational channel of the Columbia River sweeps past the study area and is in closer proximity to the shoreline here than in any other part of the river. This allows for excellent viewing of the large commercial vessels that travel the river.

At peak ebb tides, river currents average 2.5 knots. Resulting from the volume and velocity of river flow, certain areas along the riverfront are subject to severe scouring: with the area between 7th and 9th Streets being perhaps the worst affected. At this location water depths average 60-70 feet, mean lower low water, making maintenance of existing structures and new construction difficult and expensive.

The planning implications are clear. Any new pile-supported over water structures should be discouraged where current scouring is strongest. In addition, new public water level access points should be developed away from the swiftest currents for safety of

visitors and to eliminate potential damage to transient boat moorage.

Transient moorage could be provided on a daily basis at sites between Sixth and Twelfth Streets provided that the orientation of the boats is parallel to normal currents. For longer term moorage, a site near the Maritime Museum would provide the quieter water needed to prevent damage to breakwaters and moored vessels. Such moorage could also be located from the Astoria-Myer Bridge west, where more protected water exists. However, the usefulness of a transient moorage is limited at this location due to it's remoteness from downtown destinations of boaters.

Mogler



Columbia River

The 100-year flood elevation along the Astoria waterfront is 11.4 feet above mean lower low water, meaning it would crest at a point 4-5 feet below the usual downtown pier level.

BURLINGTON NORTHERN RAILROAD RIGHT-OF-WAY

The Astoria Line is a branch line of the Burlington Northern railroad and extends from northwest Portland to the Port of Astoria at Smith Point. The rail line west of Astoria has been abandoned and the tracks have been removed.

The Astoria Line right-of-way follows Astoria's Columbia River waterfront from Tongue Point to Smith Point. The right-of-way in the project area is 50 feet in width and has a single line of track which runs along the centerline. Within the project area today, train traffic, often limited to the engine only, averages 1 trip per week. Almost half of Astoria's right-of-way is built over trestles which are considered high maintenance. Burlington Northern presently is not aware of any trestles which are unsafe.

The right-of-way serves as an east-west circulation path for vehicles entering the waterfront area and for pedestrians. Gaining public access to the right-of-way for pedestrian use is a key component of the waterfront concept plan.

A majority of the buildings between 6th and 14th Street are located on or within a few feet of the right-of-way and several waterfront businesses have leased portions of the right-of-way contiguous to their property.

Though limited, there currently is rail service to the Port of Astoria. Present and future access to rail service is considered very important by the Port.

The Port of Astoria receives light, incidental train service. The major cargo moved over the past year has been hardwood logs for export. If rail traffic continues to diminish, a possibility exists that the Burlington Northern may apply for abandonment of the line.

A mechanism for reuse of rail rights-of-way called Rail Banking could be applied to the rail corridor between Tongue Point and Smith Cove. In March of 1990, the Supreme Court upheld the rail banking as a legal device which allows for the temporary conversion of railroad corridors into public rights-of-ways while guaranteeing railroads the right for possible future use. This prevents enforcement of reversionary land ownership clauses which have plagued similar public uses in other locations. The specific law establishing rail banking is the Interstate Commerce Commission's regulation implementing Section 8(d) of the Trails Act.

There is no insurmountable conflict between the Port's continued need for rail access and the public's potential use of this right-of-way. Joint use is not dependent on elimination of rail traffic. The Trails Act provisions would be used only if Burlington Northern chose to apply for abandonment.

If section 8(d) is invoked, the party asking for rail banking must agree to

take full responsibility for management and legal liability arising out of the transfer or use of the right-of-way. The applying party would be the City of Astoria.

In any agreement for joint use of the railroad right-of-way, the Oregon Public Utilities Commission would be involved as well as the Federal Rail authorities. Pursuant to Burlington Northern approval, pedestrian travel within the right-of-way may not require any separation from the tracks, other than a hand rail, due to low volume of traffic, low train speed and adequate signalization. Burlington Northern would require fencing on demand, at the City's expense, if traffic were to increase or new hazards become apparent.

Minimum clearance for train operations on right-of-ways is 8-1/2 feet from either side of the track centerline. A 25-foot easement centered on the track would allow for vehicles to pass on either side.

CLIMATE

Astoria's weather is generally classified as typical of a northwest coastal marine climate, though the only thing that can be truly said of Astoria's weather is that it is changeable. The area has some of the cleanest air to be found within the United States and receives an average of 70 inches of rainfall annually, much of which occurs during October, November, and December.

Prevailing winds are from the northeast

in summer months and from the southwest during the winter season.

VEGETATION AND TOPOGRAPHY

With the exception of a few evergreens located directly west of the Columbia Maritime Museum and the recently developed streetscape along 10th Street, there are no areas of notable vegetation within the project area.

The waterfront area is predominately flat with little or no topographical differences.

Presently the shoreline is eroding between 11th and 12th Streets.

STREETSCAPES

Recently, 10th Street between the river and Marine Drive was developed as a model streetscape program. Improvements consisted of light standards, raised concrete planters with street trees, small planting areas at street corners, and benches.

Other streets within the waterfront and those connecting to the downtown core have received little improvement within recent years. Many streets within the area are of chair-wall construction which will limit the number of street trees that can be planted. Pedestrian crossings at Marine Drive and Commercial are minimally marked.

Streetscapes along the following routes will help to link the project area with

the downtown district: 8th, 10th, 11th, 12th, 14th Streets and 17th Street to the Heritage Museum. The sidewalks along these routes need to be enhanced with improvements such as furniture, landscape, lighting and art objects. These routes need to include places to pause, rest, and watch activities of the city and waterfront. They need to become places that improve the pedestrian experience and incorporate street furniture that is convenient to use and in character with the City and waterfront.

HISTORIC LANDMARKS

In and around the waterfront area are landmarks of Astoria's rich and colorful past. The foot of 9th Street is the former location of Hustler's Wharf which was built in 1870 and was Astoria's first large ship dock. The foot of 11th Street gives the best example of the pile supported wood planked streets of early Astoria.

Astor Street begins what was once Astoria's red light district or Swilltown.

At the foot of 14th Street was the site of the Astoria-Megler Ferry Landing which was in operation from 1925 to 1966.

The foot of 15th Street marks the location of Astoria's first paved road built in 1915.

Other points of historical/cultural interest within walking distance of the waterfront include:

- o Flavel Mansion
- o County Courthouse
- o Tidal Rock
- o Fort Astoria
- o John Jacob Astor
Apartment
- o Liberty Theatre
- o Heritage Museum
- o Columbia River
Maritime Museum
- o Svenson Blacksmith
Shop
- o Site of first Post
Office west of the
Rockies.

A more detailed account of Astoria's history is available in the Historic Context Astoria Downtown Area by John Goodenberger and Bonnie Othes, 1990. The downtown area may be designated as a nationally registered historic district within the next few years.

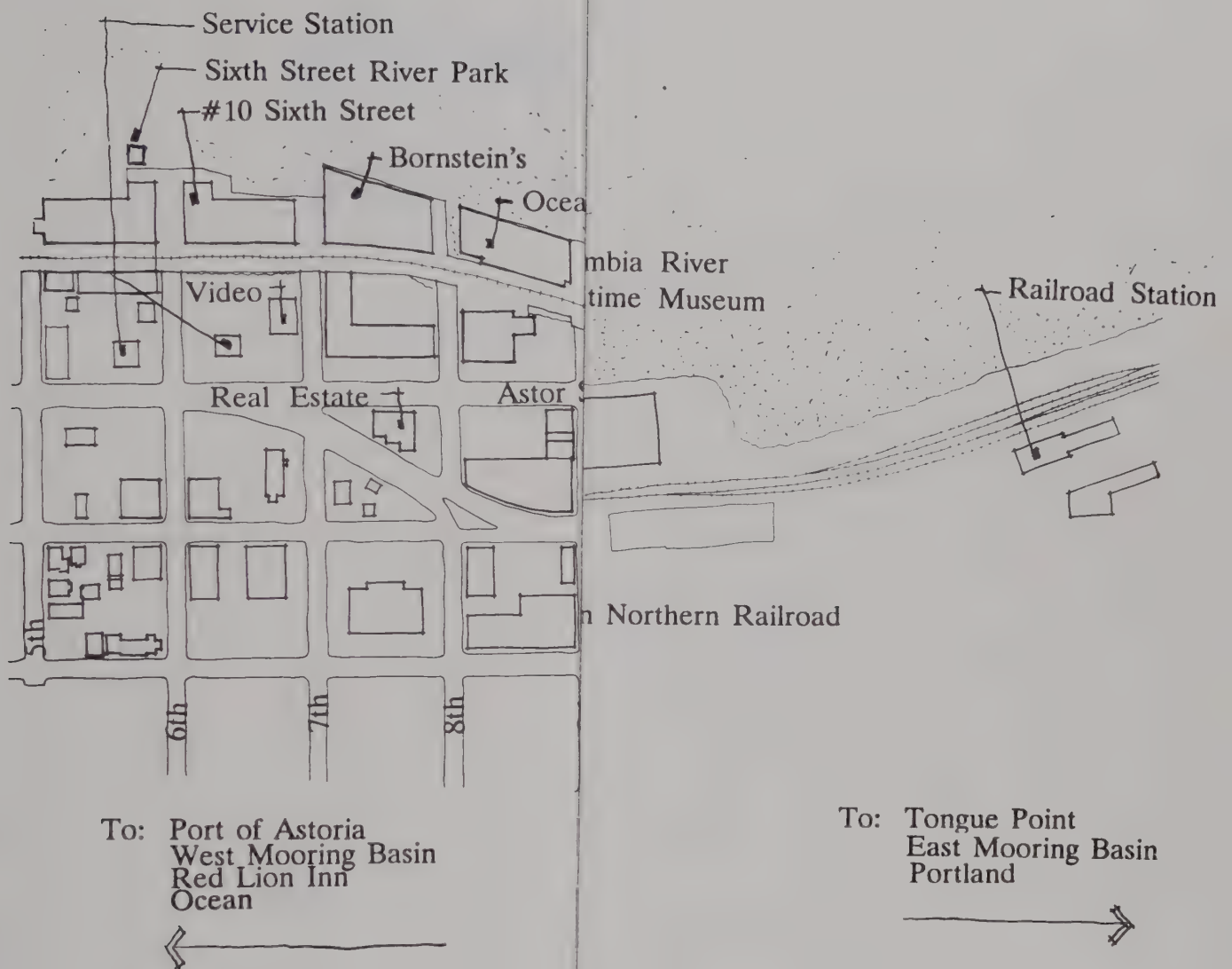
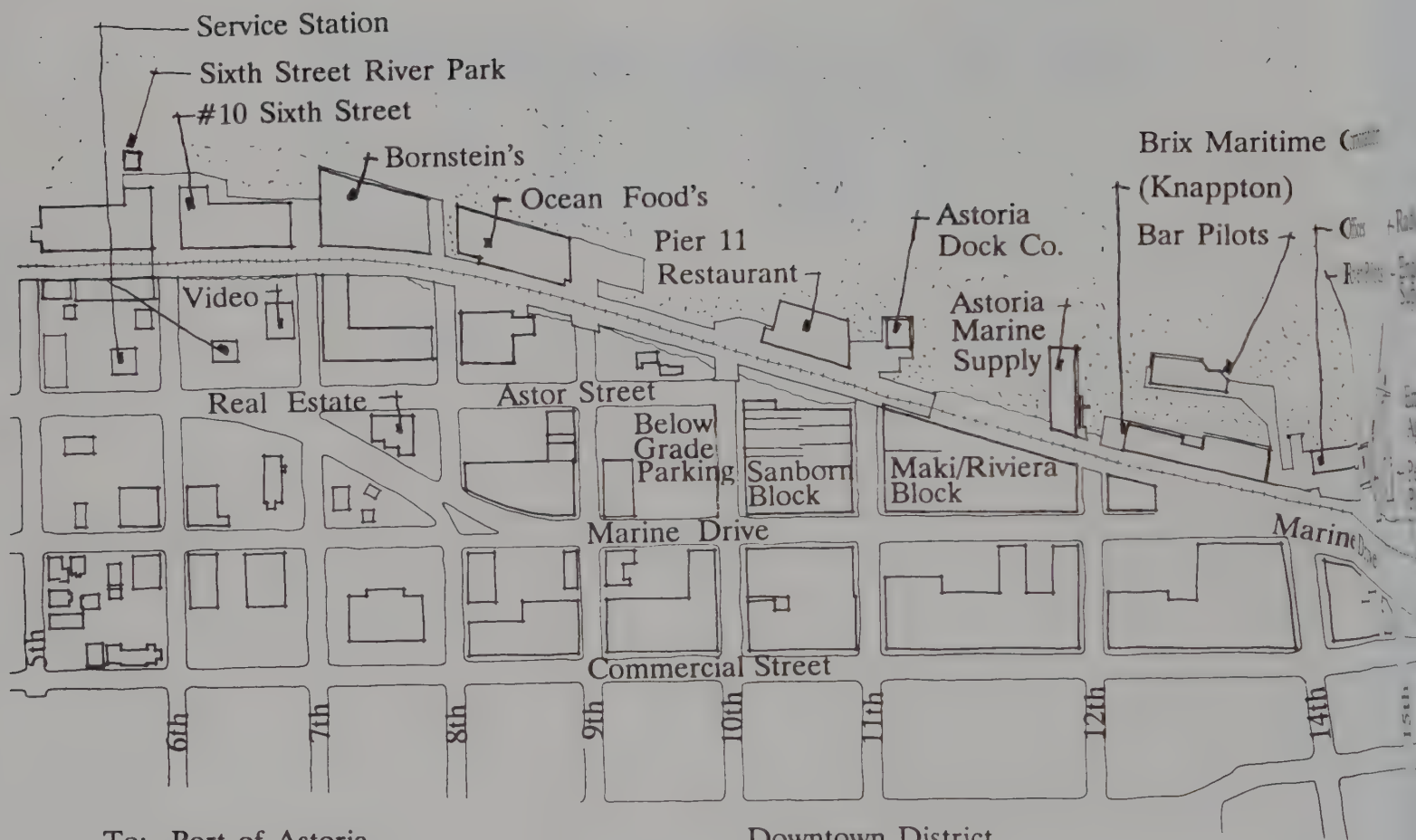


Figure 3.1

Columbia River

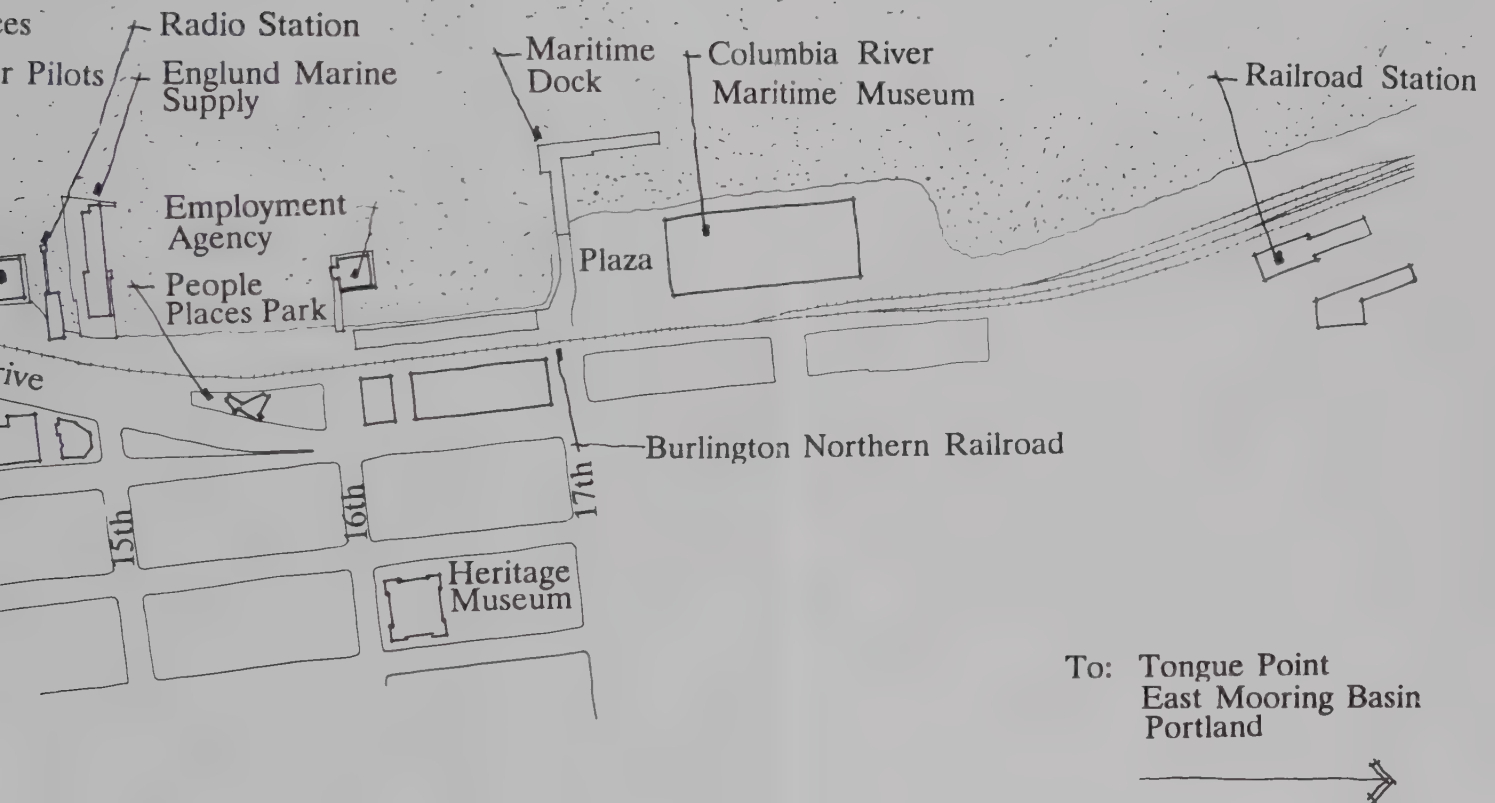


To: Port of Astoria
West Mooring Basin
Red Lion Inn
Ocean

Downtown District



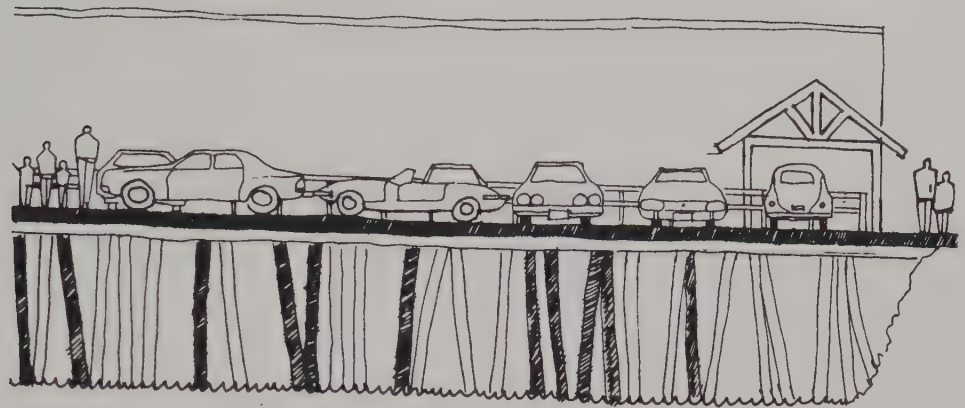
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ASTORIA WATERFRONT STUDY

Existing Conditions

Figure 3.1



NEW DECK PARKING

OPPORTUNITIES & CONSTRAINTS

OPPORTUNITIES AND CONSTRAINTS

Below is an outline of the extent and occurrences of cultural and natural factors which, either singularly or in combination, present assets or liabilities in the master plan planning process.

Many of the cultural and natural factors for the Astoria waterfront can be either opportunities or constraints, depending on the particular design concept, approach or point of view.

5th to 7th Street

Opportunities

- o Increase existing parking area between 6th and 7th Street.
- o Incorporate parking within railroad right-of-way west of 6th Street.
- o Develop restrooms in existing brick structure west of 6th Street.
- o Pedestrian path potential along north side of right-of-way.
- o 6th Street River Park acts as western anchor of project area.
- o Potential for riverside restaurant location within No. 10 Sixth Street.
- o Kinney Warehouse, west of 6th Street and on the Register of Historic Places, could be used for tourist-oriented commercial purpose.
- o Panoramic view of river and ship traffic from Sixth Street River Park.

- o Potential for parking and restroom at gas station site between 6th and 7th Streets.

Constraints

- o Buildings developed on edge of right-of-way.
- o Storage, associated with Bornstein's Seafood, blocks views at foot of 7th Street.

7th to 9th Street

Opportunities

- o Path potential on the north side of right-of-way.
- o Astor Street's unique history and qualities have potential for revitalization.
- o Panoramic view of river, Washington shoreline, and boat unloading at 8th Street terminus.
- o Fish processing facilities have "cannery row" appeal to tourist.
- o Streetscape improvement from river to commercial on 8th and Astor Streets.
- o Western gateway into waterfront area.
- o Employee parking lot west of 8th Street, capacity and visual quality can be enhanced.

Constraints

- o Pedestrian crossing difficult at 7th/8th Streets and Marine Drive.

- o Conflict (liability, productivity) between fish processing operations and tourist walking through area. Signage identifying working area and other precautions may be necessary.
- o Debris under railroad tracks needs removal.
- o Buildings developed on right-of-way line.
- o Fish processing facilities presently employ approximately 200 people and expansion is a possibility. Parking within project area needs to be provided for these employees.
- o Deep water close to shore caused by scouring effect of river channel.

9th to 10th

Opportunities

- o Path potential along north side of right-of-way with expansive views to river.
- o Deck area west of Pier 11 in public ownership offers possibility for public viewing and interpretive signage.
- o Historical significance, potential interpretive display:
 - Hustler's Wharf - first large ship dock - 1870.
 - 9th & Marine Drive - main commercial intersection of the 1880's and 1890's, and
 - 9th & Aster Street, beginning of Astoria's red light district.

- o Parking area west of 10th Street is not fully utilized and has potential for expansion.
- o Pacific Power & Light Substation has potential for enhancement and examples of original City of Astoria lamp standards. Area could be rezoned as open space in anticipation of possible future substation relocation.
- o Restoration of building frontages along 10th Street.
- o Expansion of 10th Street pilot streetscape program.

Constraints

- o Broken view from foot of 9th Street.
- o Recently completed Ice House obstructs view from 9th Street terminus. ✓
- o Spaces within parking area west of 10th Street are leased and are not accessible to public.
- o Deck west of Pier 11 presently used for restaurant parking and needs protection from wind and rain.
- o Pedestrian crossing at Marine Drive needs identification.

10th to 11th

Opportunities

- o Path potential along north side of right-of-way.
- o East of 10th Street much of the shoreline riprap was hand placed.
- o Astor Street provides natural route to leave right-of-way.

- o Potential net repair dock locations east or west of Pier 11.
- o Deck area east of Pier 11 in public ownership.
- o Potential expansion could provide for increased parking, public view point, interpretive area, and view point for fish processing facility west of Pier 11.
- o Building is built south of the right-of-way line and has potential to orient towards waterfront. Presently western 1/2 of building has been developed for galleries and other commercial uses. Frontage has architectural interest.
- o North side of building could be developed with view decks and restroom.
- o Historical significance, potential interpretive display:
 - 11th & Marine Drive is where the fire of 1922 started, and
 - Foot of 11th gives best sense of early Astoria planked streets.

Constraints

- o Erosion presently occurring along Sanborn block.
- o Pier 11 needs parking areas presently used, and building is developed on right-of-way line.
- o Building does not take advantage of river views.
- o Lack of public restroom along waterfront a problem for waterfront restaurants.
- o Deck east of Pier 11 is in need of repairs; slickness has caused

accidents.

11th to 12th

Opportunities

- o Pedestrian path along north side of right-of-way could offer open views of river.
- o Allied Van may relocate permitting a more water-oriented use to occupy space and relieving truck traffic from shoreline.
- o Potential area to extend out onto the river for public view point.
- o North side of Maki/Riviera Block, adjacent to railroad right-of-way, offers potential for open deck areas with excellent views of waterfront activities.
- o Streetscape along 11th Street could strengthen pedestrian connection to downtown.

Constraints

- o Trucks are currently parked along right-of-way behind Allied Van.
- o Buildings located on right-of-way line.
- o Building facade south of right-of-way has little visual interest.
- o Approximately one half of building south of right-of-way is vacant and in need of repair.
- o Severe erosion along shoreline.

12th - 14th

Opportunities

- o Pedestrian path adjacent to north side of right-of-way.
- o Brix Maritime Corporation Buildings provide sheltered area for potential downtown moorage.
- o Building closest to right-of-way is structurally unsound and provides opportunity for redevelopment or removal. Pilings are in good condition and a percentage of the building is presently used for parking.
- o River pilot, bar pilot and tug activity provide excellent viewing for visitors.
- o Historic Significance, potential interpretive display:
 - Foot of 14th Street former ferry dock,
 - First view of Tongue Point from this area, and
- o Streetscape along 12th Street could strengthen pedestrian connection to downtown.

Constraints

- o Scouring from current is problem within area.
- o Brix Maritime Corporation building blocks view to river from Marine Drive.

14th - 15th

Opportunities

- o Path potential north of right-of-way.
- o Streetscape along 14th Street could strengthen pedestrian connection to downtown.

Constraints

- o Visual character of buildings does not conform with waterfront character.
- o Views to river are blocked by buildings.

15th - 17th

Opportunities

- o Path potential north or south of right-of-way.
- o Panoramic views of waterfront,
- o People's Park adjacent to south side of right-of-way offers views to river.
- o First views of river from Marine Drive.
- o City of Astoria owns parking lot west of Museum. Spaces are leased to the United States Coast Guard.
- o Potential for floating net repair area east of Englund Marine Supply.
- o Potential for interpretive displays along pedestrian path.
- o Existing parking can be realigned to accommodate pedestrian path.

Constraints

- o Two-way vehicular circulation needs to be maintained for business operations.
- o People's Park needs stronger connection to shoreline.
- o Evergreens along shoreline block views to river from Marine Drive.

17th To Eastern Edge Of The Columbia River Maritime Museum

Opportunities

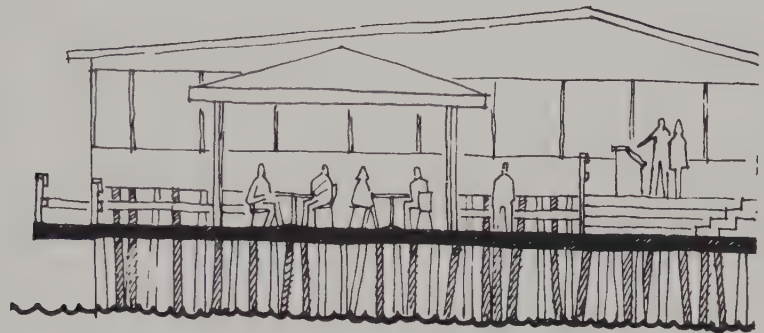
- o Main entry point to Museum.
- o Streetscape improvements would strengthen pedestrian connection between Heritage Museum and downtown.
- o Expansive views of river from the museum plaza.
- o Museum presently has 100,000 visitors a year and anticipates an increase of up to 150,000 to 200,000 visitors within the next 3 years.
- o Museum may expand within the next 5 years, plans include extended public dock, moorage area and amphitheater.
- o Provide access to river for visitors.
- o Museum recently acquired the Burlington Northern station and an existing trailer park.
- o The Burlington Northern station may be developed with tourist facilities.
- o Existing viewing areas have potential for interpretive displays.

Constraints

- o Museum parking is insufficient and visitors remain parked on museum lot while they visit other downtown locations.
- o Buses and recreation vehicles require more parking space and a designated parking area is needed.
- o Signalization is needed at 17th

and Marine Drive for vehicles entering the museum and pedestrians crossing to the Heritage Center.

- o Large recreational vehicles have problems entering facility due to their turning radius.
- o Pedestrian crossing is dangerous across Marine Drive.



EXPANDED DECK

SHELTER

KIOSK

MASTER PLAN

MASTER PLAN PURPOSE

The Master Plan that has been developed is both a vision and a program for the future of the downtown riverfront. This plan gives a physical form to the thoughts and ideas that were expressed by the CAC, TAC, APO, City officials and the general public during the overall waterfront planning process. The intent of these drawings is to establish a framework which will ensure the consistency of individual designs within the waterfront in relationship to identified public objectives and development regulations.

This plan will provide the overall program and general design requirements for projects to be undertaken by the City and for those portions of the waterfront to be developed by the private sector. Final architectural or landscape solutions for each specific project within the waterfront will not be provided in this phase of work. This will be the responsibility of subsequent, more detailed design studies.

MASTER PLAN PROCESS

A collaborative process was maintained between City officials, the CAC, TAC, APO, concerned citizens of the Astoria community and the design team from the project's onset in early spring of 1989.

In May, 1989, the planning effort began formally with two walking tours through the project area. The first tour was comprised of APO members, City officials, and the design team. The second tour, held later the same day, consisted of the CAC, TAC, City officials and the design team. The intent of each tour was to familiarize all of the decision makers with each other and the site, and to establish project goals.

The first public meeting was held in July of 1989 to review a summary of existing conditions within the project area and preliminary design concepts.

The second and third public meetings were held in October of 1989 to review alternative plans that were developed based on the initial public meeting. The second meeting was an open house, and specific ideas were discussed with the CAC, TAC, APO, and the general public on a one-by-one basis. The third public meeting, held two weeks later, reflected the changes in ideas and thoughts brought out at the open house.

A fourth public meeting was held in December of 1989 to review the final

draft master plans developed for the downtown riverfront area. The format of the meeting was that of an open house, and plans were once again reviewed on a one-by-one basis with concerned citizens and committee members.

The project concluded with a presentation to City Council in June of 1990.

MASTER PLAN SUMMARIES

GENERAL

One master plan with 2 alternative schemes, Alternative A and Alternative B, has been developed for the downtown riverfront. Alternative approaches have been developed due to the uncertainty of future pedestrian and vehicular access to a portion or all of the Burlington Northern right-of-way.

Alternative A demonstrates the potential the waterfront has if the City is able to gain access to the right-of-way area. An immediate pedestrian path and a future trolley route along the Burlington Northern Railroad tracks are the distinctive elements of this plan.

Alternative B illustrates the possibilities the waterfront has if the City is unable to utilize the right-of-way for public access. An improved pedestrian path, that enhances the existing system would be a feature of this concept.

Though the overall master plan concept is broad in scope, the individual components consist of small, intimate areas. When pieced together, these pieces will bring new life and vitality to the waterfront while maintaining and enhancing its present positive attributes. To actualize this goal, each block within the project area will be developed to its full potential according to a comprehensive and cohesive plan.

With this guiding concept, the following proposed improvements are explained on a block-by-block basis. As this report proceeds areas of special significance are reviewed in more detail.

The success of the project will depend on the ability of future improvements to meet the objectives that have been established by this study. To ensure a viable waterfront, an assertive effort will need to be made by the City to identify the project area as a special planning district and to adopt supportive design and planning guidelines. Incorporation of design and planning guidelines will:

- o ✓ protect the existing working waterfront ambiance,
- ⑥ improve the area's visual identity,
- o identify and maintain important view corridors,
- o help to revitalize commercial activity within the waterfront area and the adjacent central business district, and
- ⑥ improve the overall quality of all new development.

This would be accomplished through establishment of detailed and site specific policies for the following guideline components:

- ⑥ building height limits,
- ⑥ building setbacks, materials, and massing,

- o signage,
- o architectural character,
- o right-of-way development and streetscapes,
- o pedestrian sidewalk widths and materials,
- o bikeway design,
- o courtyards and plazas,
- o piers and waterfront structures,
- o lighting,
- o street furniture,
- o utilities,
- o landscaping, street trees and
- o parking lots.

It is essential that the City of Astoria establish a special design district ordinance, complete with appropriate guidelines to manage the function and design of specific blocks within the downtown waterfront area to ensure that this vision becomes a reality.

ALTERNATIVE A

Alternative A is the preferred plan of the groups and individuals involved with this study. The plan is based on the assumption that the City and the Burlington Northern Railroad will resolve the issue of public access through the right-of-way.

The possibilities of incorporating a trolley/ rubber tired vehicle system through the project area are explored in Alternative A. This system could provide linkage between the various waterfront components and a festive continuity to the district. Due to the economic considerations involved with implementation, the trolley is considered to be a future-oriented concept by all those involved in the planning process. Although a trolley would be enjoyed by residents and would attract visitors, the success of Alternative A is not dependent on it.

5th to 6th Street

- o West of 6th Street, immediately north of the existing metal building, and within the railroad right-of-way, presents an opportunity for additional parking and enhanced traffic circulation.
- o If the privately owned property west of 6th Street were ever to become available and purchased by the City, the existing metal frame building located within the northern half of the block area could be redeveloped as a 31 car parking area. Due to their

unique historical character, the two small existing brick buildings could be redeveloped. The brick building on 5th Street could serve as the western trolley station and/or provide needed public restrooms. The open sided brick structure on 6th Street could be revitalized as a future commercial use. To buffer the parking area, a 10-foot wide landscape area could be incorporated along the site's northern and eastern edge.

- o A pedestrian crossing would be incorporated at the intersection of 6th Street and Marine Drive to facilitate pedestrian circulation.



Proposed Trolley

6th to 8th Street

- o Should the gasoline service station between 6th and 7th Streets become available, the area could be redeveloped as an

80-car lot to accommodate the need for additional parking. A 10-foot wide planting area for screening would be incorporated around the parking area. The video store would remain in its present location.

- o A 1,500 square foot wood-planked deck could be built at the northeast corner of #10 Sixth Street in conjunction with development of a future restaurant. The deck could provide an outdoor dining area for the future restaurant.
- o A 400-square foot river viewing area would be built at the foot of 7th Street. To eliminate any potential vehicular/pedestrian conflict, the view area would be raised. An interpretive kiosk would be incorporated within the viewing area.
- o The intersection of Marine Drive and 7th Street is intended to be the western gateway into the waterfront. The triangular area west of the existing real estate office could be embellished with landscaping, signage indicating the riverfront area, and a pedestrian access route across Marine Drive.
- o The unimproved parking area south of Bornstein's Seafood Processing Plant would be redeveloped as a 24-car private lot. A landscape buffer would be incorporated along the north side of the space for screening.
- o To invite shoppers and visitors to stroll from one end of Astor Street to the other end, enrich

the existing character, bring the street into scale with the pedestrian and strengthen the area's identity, streetscape improvements are recommended from 7th to 10th Street.

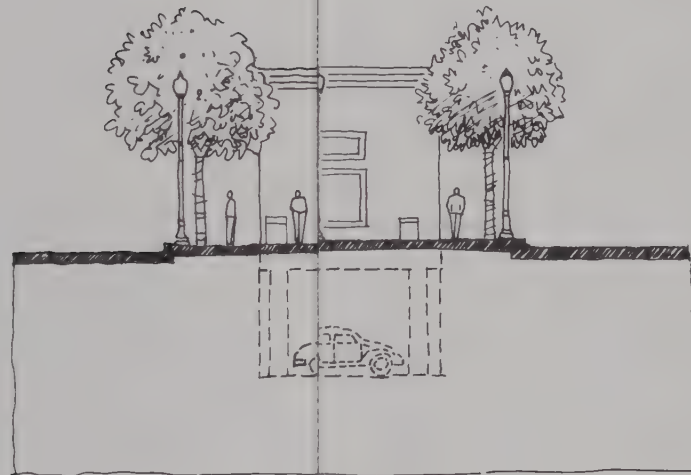
Streetscape improvements could include: resurfacing of sidewalk areas and Astor Street itself with pre-cast concrete pavers that would have a brick-like appearance, street furniture (benches, litter receptacles, bollards and informational signs that are in character with the revitalization project, incorporation of lighting standards that are complementary to the newly placed fixtures along 10th Street, street trees and tree grates, screening of existing parking areas with plant material, signage which is historically appropriate and building paint colors that would tie Astor Street together as a unit. Specialty shops, professional offices, and apartments could be encouraged to bring people and activity to the area.

- o To provide east/west pedestrian circulation, a 10-foot wide wood planked pedestrian path that maintains the "working waterfront feeling" would be located within the northern side of the railroad right-of-way. A 36-inch handrail would extend along the north and south side of the path. Lighting standards spaced approximately every 50 feet are proposed to yield adequate illumination for security and safety, yet provide a



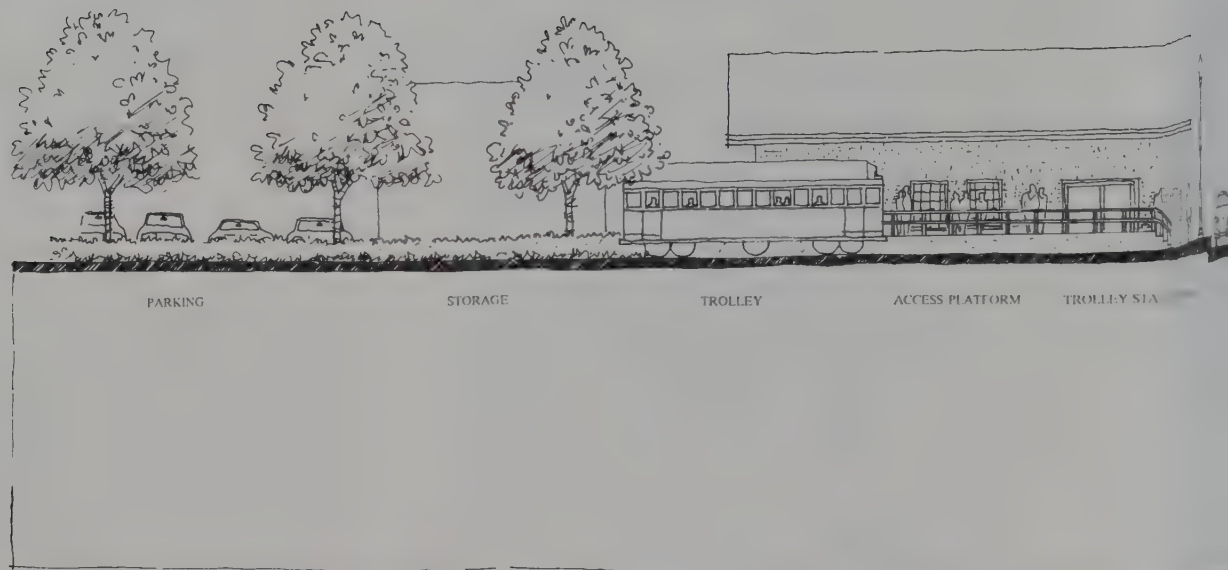
PARKING

WEST TROLLEY



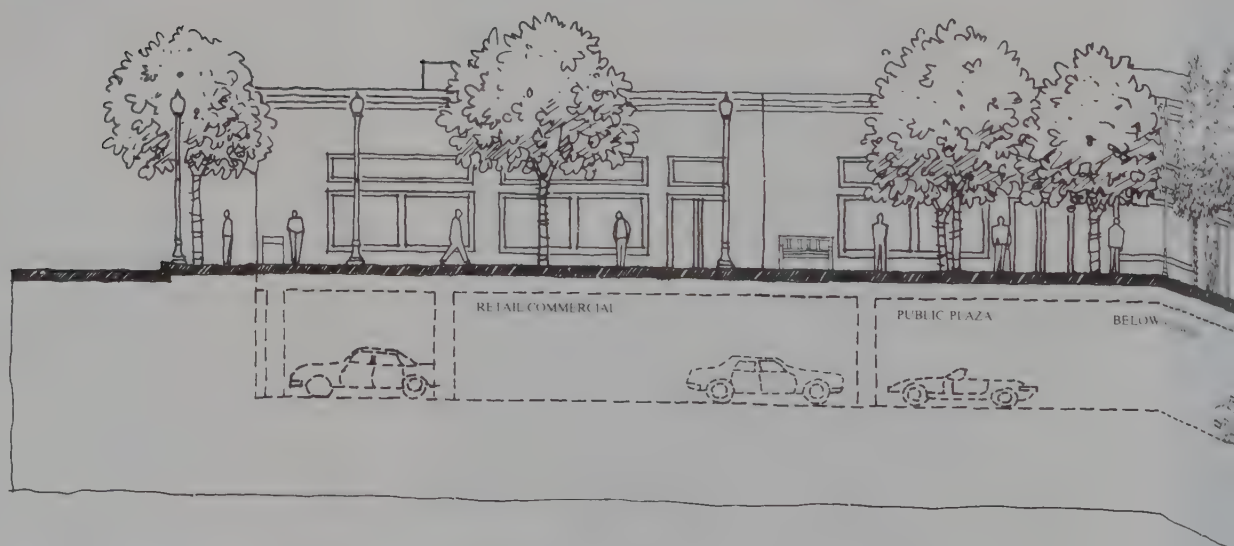
ASTOR STREET

Figure 5.1



WEST TROLLEY STATION SECTION

SCALE: 1/8" = 1'-0"



ASTOR STREET COMMERCIAL/PARKING COMPLEX SECTION

SCALE: 1/8" = 1'-0"

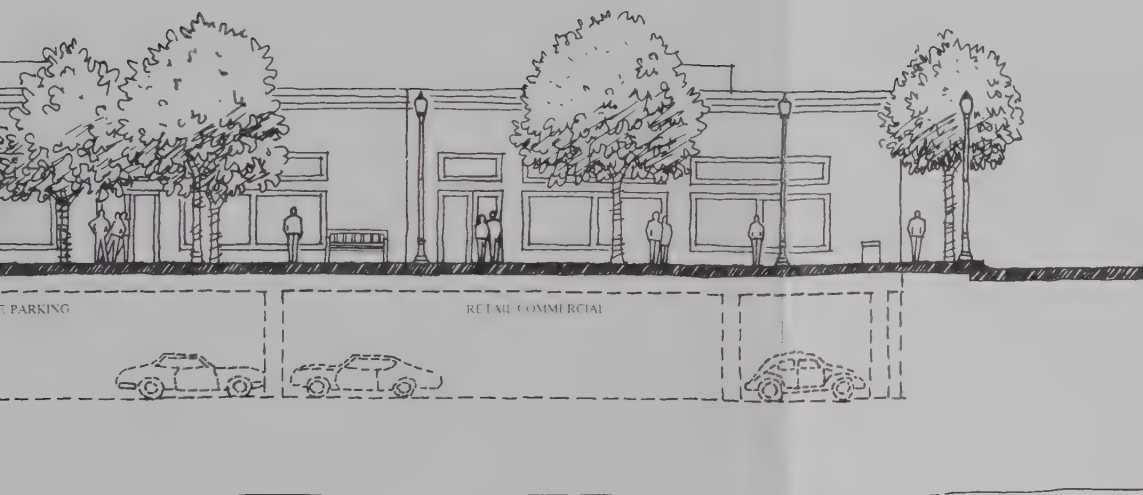


Figure 5.1

soft light consistent with the historic character. The section of the path parallel to Bornstein's would be marked with signage explaining the need for pedestrian caution, and non-skid surfaces would be provided where necessary.

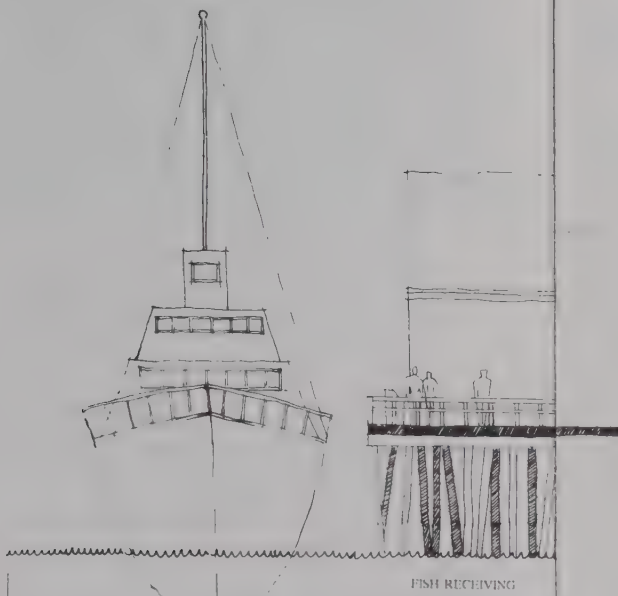
8th to 9th Street

- o A catwalk separate from Ocean Food Seafood Processing Plant would be developed, at the foot of 8th Street, along the west side of the building and terminate in a 100-square foot view area. However, with the cooperation of the adjacent Ocean Food Seafood processing facility, an alternative of cantilevering exists.
- o A 10-foot wide wood-planked pedestrian path would be located within the north side of the right-of-way. This path would replace the existing concrete surface that presently exists. Signage explaining the need for pedestrian caution and non-skid surfaces would be provided where necessary.
- o Due to limited visibility, pedestrian circulation at the intersection of 8th and Marine Drive should be discouraged. The existing triangle could be resurfaced with brick pavers and enhanced with a sculpture complementary to the existing character of the area.
- o 8th Street from the river to Commercial would be enhanced with a new streetscape. Improvements would include

lighting standards, street furniture, and landscaping.

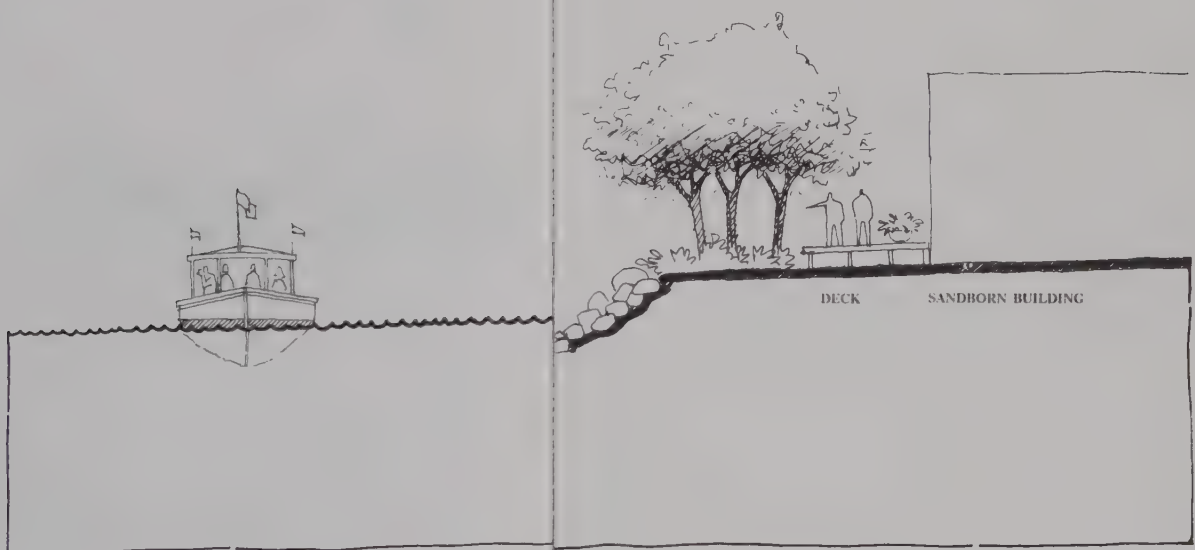
9th to 10th Street

- o Redevelopment of the existing parking area at 10th and Astor Street presents a future opportunity for redevelopment. The site has the potential of becoming a 30,000 square foot specialty shop, professional office, and apartment complex centered around a 14,000 square foot public plaza with below-grade parking.
- o The existing deck area west of Pier 11, at the foot of 10th Street would be expanded by approximately 2,500 square feet and developed with an enclosed shelter, outdoor seating and a interpretive kiosk. Parking would be prohibited except for 3 physically disabled spaces. This area could be one of the future designated waterfront trolley stops.
- o The property now occupied by Pacific Power and Light substation could be rezoned with an open space designation. If the substation were to relocate, the property could be easily developed as a riverfront park.
- o A 10-foot cantilevered wood-plank path would be incorporated north of the railroad right-of-way with railing, lighting, benches, and litter receptacles.
- o A pedestrian crossing would be incorporated at the intersections of 9th and 10th Streets with Marine Drive and Commercial to



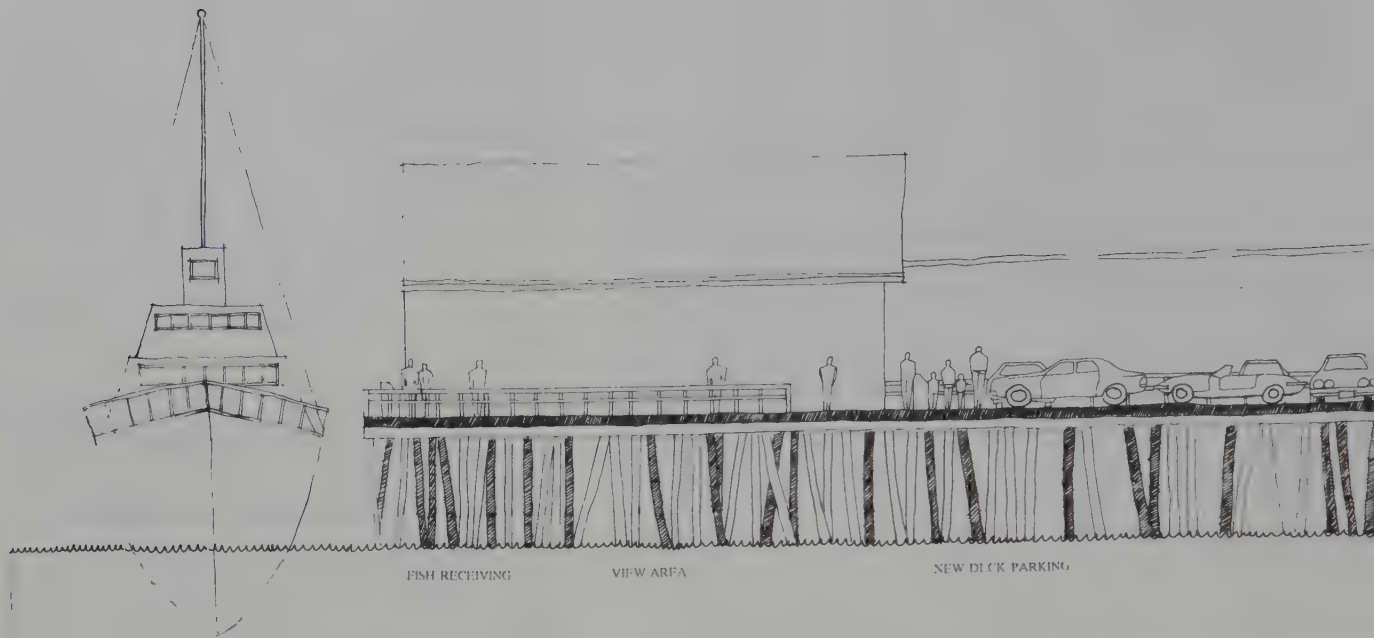
EAST PIER 11 DECK SECTION

SCALE: 1" = 10'



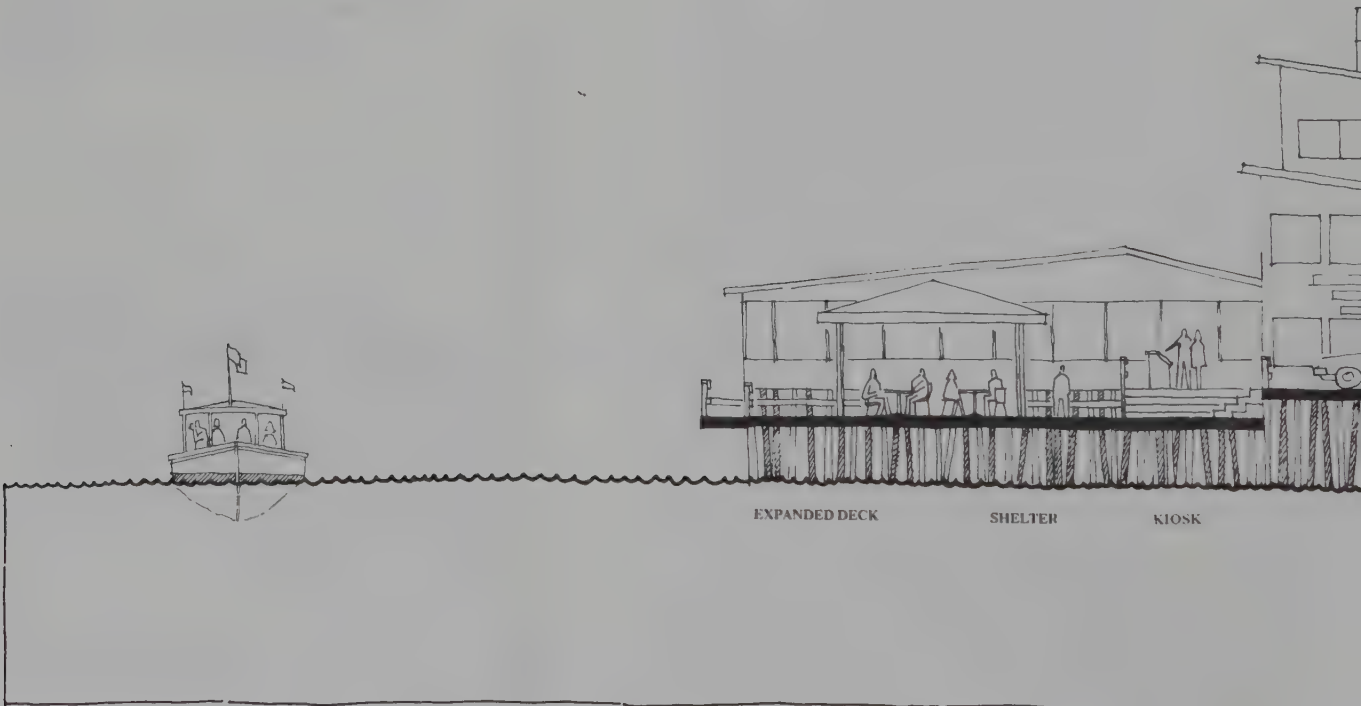
TENTH STREET VIEW AREA S

Figure 5.2



EAST PIER 11 DECK SECTION

SCALE 1/8" = 1'-0"



TENTH STREET VIEW AREA SECTION

SCALE 1/8" = 1'-0"

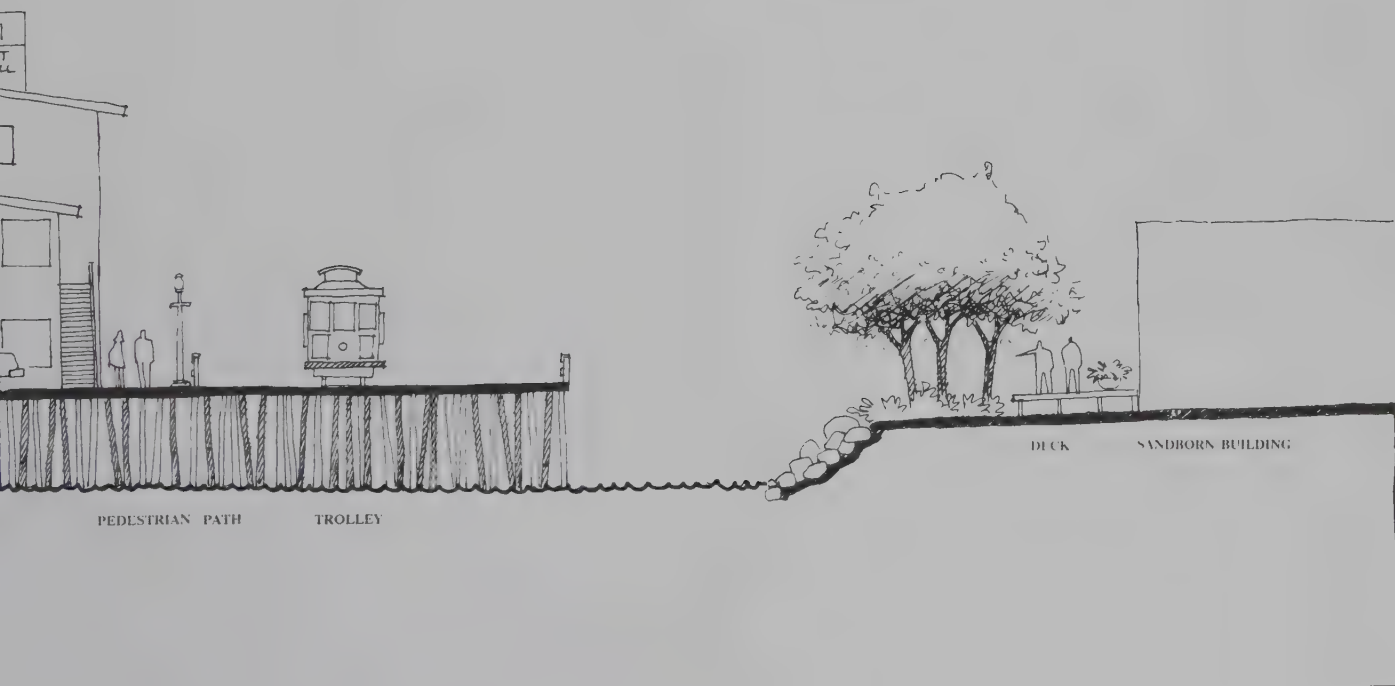
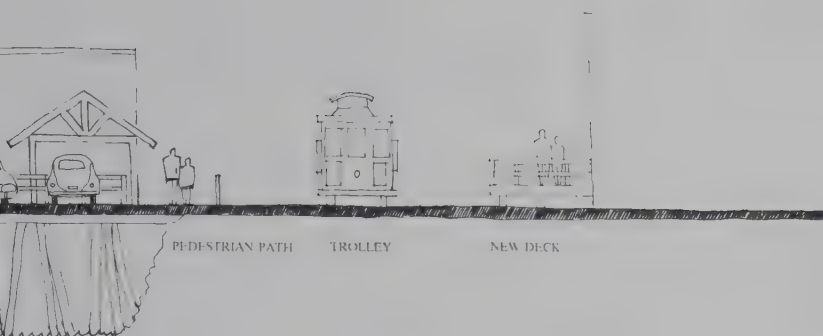


Figure 5.2

facilitate pedestrian circulation.

- o A pilot streetscape exists along 10th Street from the river south to Marine Drive. Complementary streetscape improvements would enhance 10th Street from Marine Drive to commercial. Improvements would include lighting standards, street furniture, and landscaping.

10th to 11th Street

- o A 2,500 square foot outdoor deck area would be incorporated with the permission of the property owner along the north side of the Sanborn Block for river viewing and outdoor dining.
- o The existing deck area east of Pier 11 would be expanded by approximately 4,000 square feet to accommodate present use of street end parking. The north side of the deck area would be designated as a "no parking" area to accommodate river views and adjacent fish receiving use.



Proposed Pedestrian Path



Proposed Pier 11 Deck

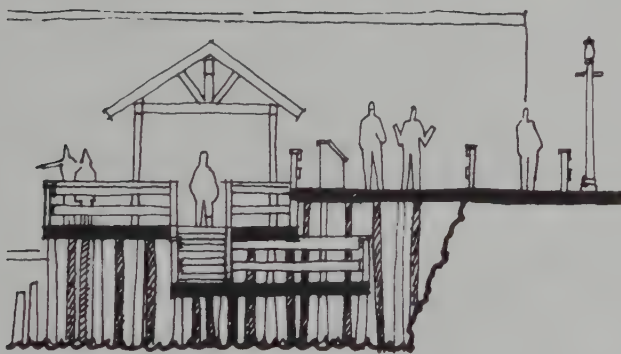
- o A 10 foot wide wood-planked pedestrian path with handrails and lighting would follow the north side of the right-of-way.
- o A pedestrian crossing would be incorporated at the intersections of 11th Street/ Marine Drive and 11th Street/Commercial to facilitate pedestrian circulation.
- o 11th Street from the river to Commercial would be enhanced with a new streetscape. Improvements would include lighting standards, street furniture, and landscaping.

11th to 12th Street

- o With permission of the property owner, a 3,500 square foot rectilinear outdoor deck could be developed along the north side of building adjacent to railroad right-of-way to accommodate outdoor dining and river views.
- o A multi-level wood deck would

be developed along the north side of the right-of-way. The area would allow visitors access to the river regardless of the tide level. A shelter for protection against foul weather would be constructed as part of the multi-level deck area.

- o A public art project that draws on the natural and historical aspects of the riverfront is envisioned here. Utilizing existing pilings, a sculpture is visualized as an extension of the multi-level deck area.
- o A wood-planked area, that would not impede vehicular traffic, would extend across the right-of-way from the outdoor deck to the multi-level water access area to encourage pedestrian movement.
- o A 10 foot pile supported wood



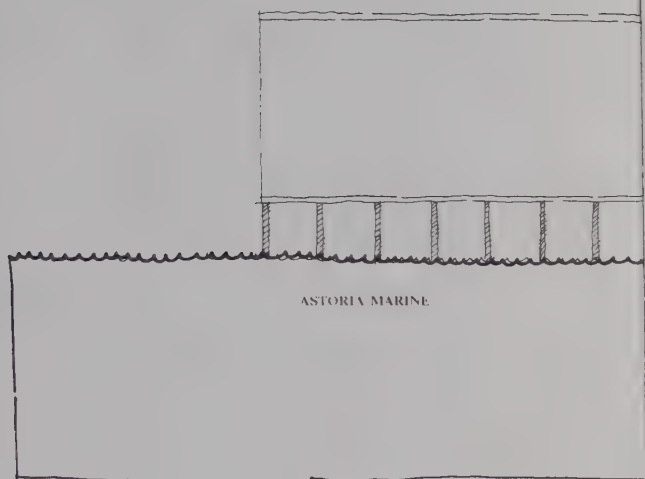
Proposed Water Access Area

plank path would be incorporated north of the railroad right-of-way with lighting, benches, and litter receptacles.

- o A pedestrian crossing would be developed at the intersections of 12th Street/Marine Drive and 12th Street/Commercial to facilitate pedestrian circulation.
- o 12th Street from the river to Commercial would be enhanced with a new streetscape. Improvements would include lighting standards, street furniture, and landscaping.

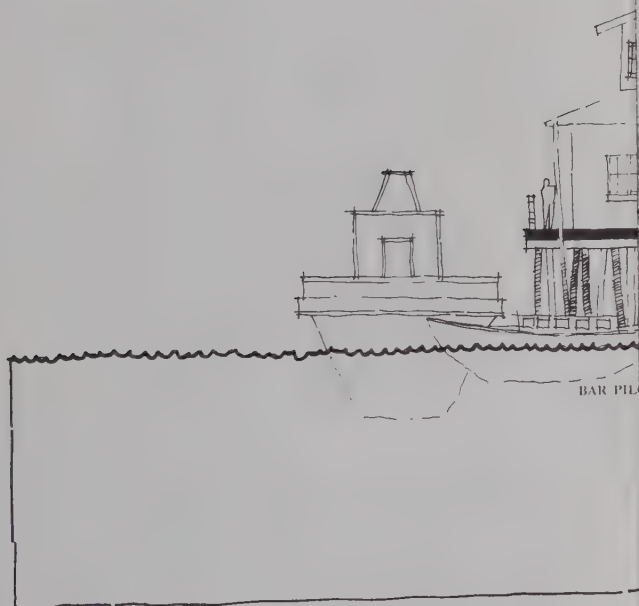
12th to 14th Street

- o The existing Brix Maritime Corporation building closest to the Burlington Northern right-of-way would be redeveloped as a 12,500 square foot deck area with a clerestory tin roof. The area is envisioned to accommodate up to 40 parking spaces and special events such as a weekend farmers market, art fairs, and a variety of public events. A restroom would be located here along with interpretive kiosks. This could be one of the future stops for the trolley.
- o A 10-foot wide wood-planked pedestrian path with railing, benches and lighting would be located within the north side of the right-of-way.
- o A small day moorage with up to six slips would be developed at the foot of 12th Street for summer day use. The moorage would be removed or floated to more protected water in winter



11TH/12TH STREET WATER ACCESS A

SCALE 1/8" = 1'0"



BRIX MARITIME CORPORATION (KN

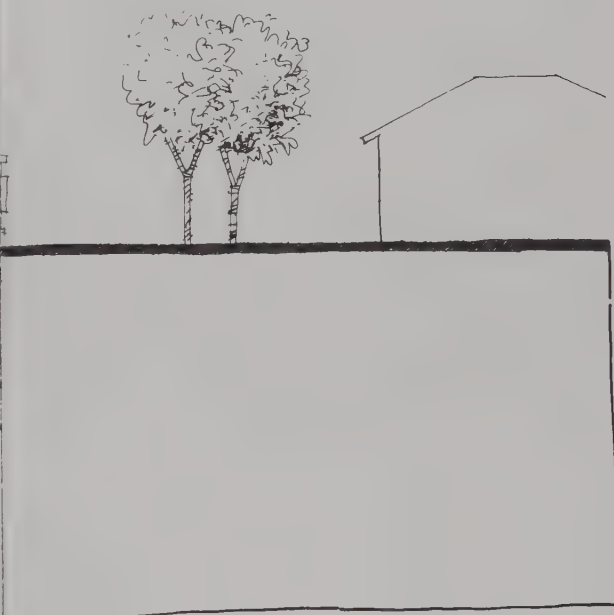
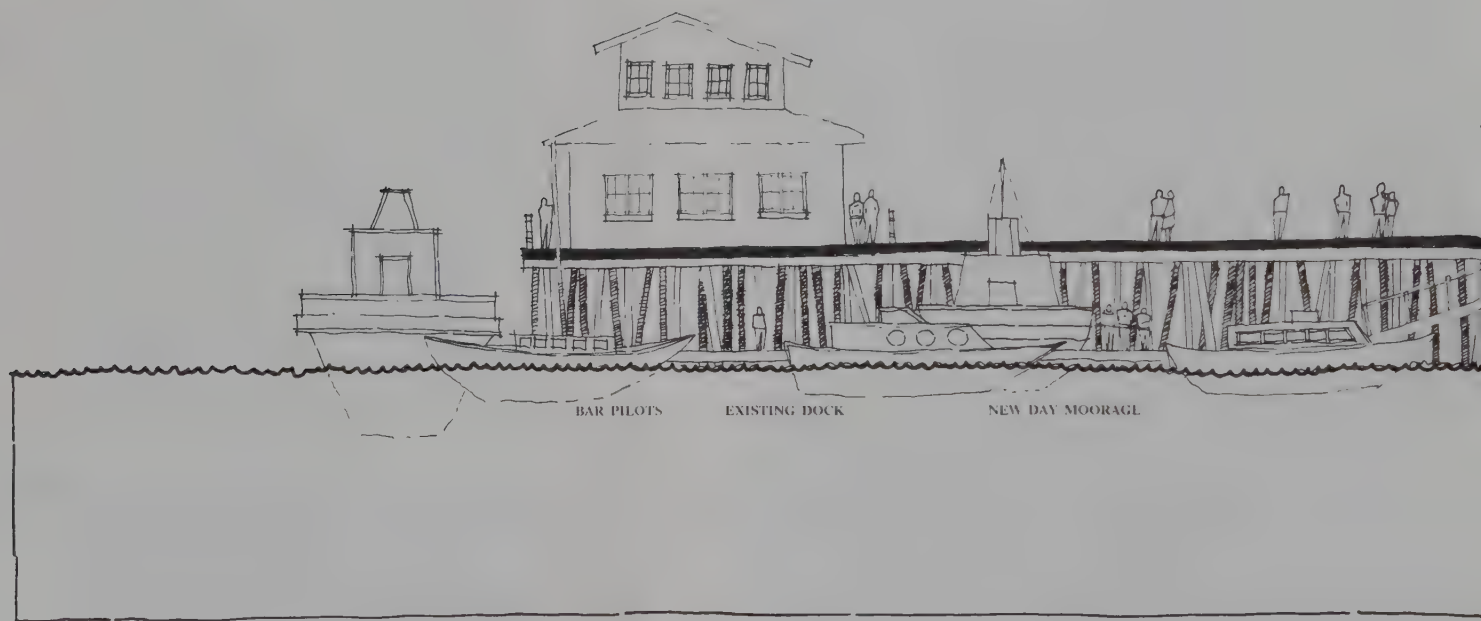


Figure 5.3



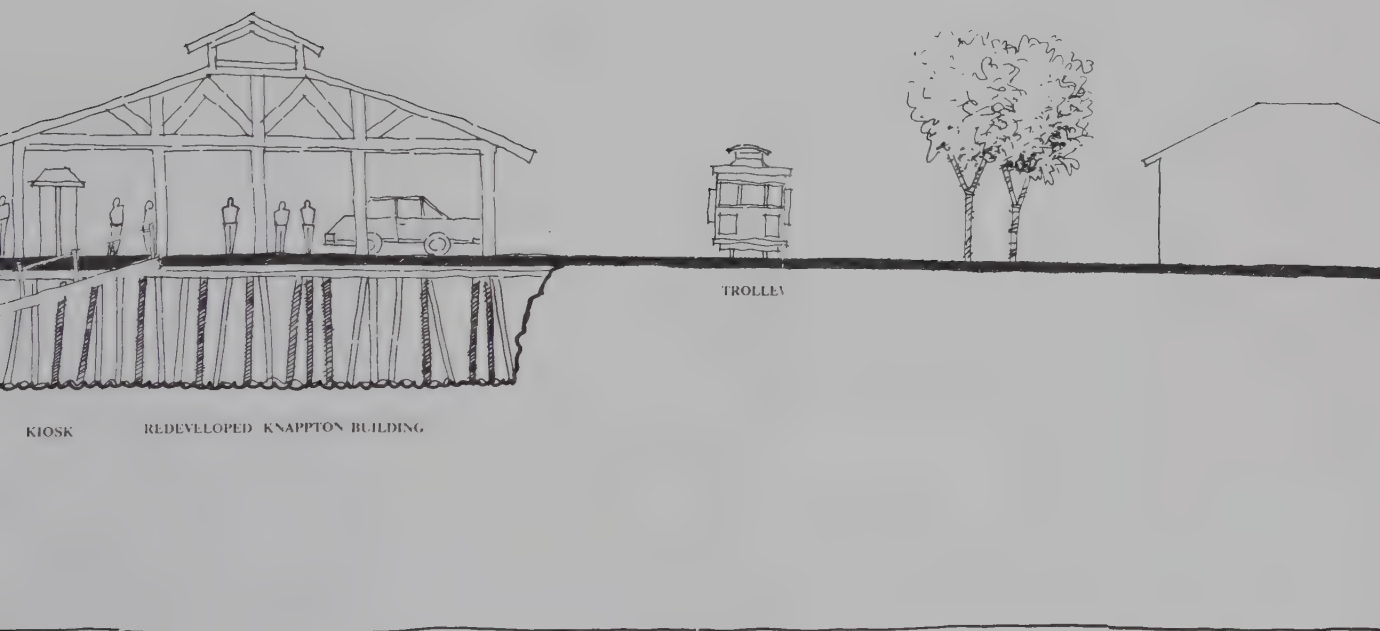
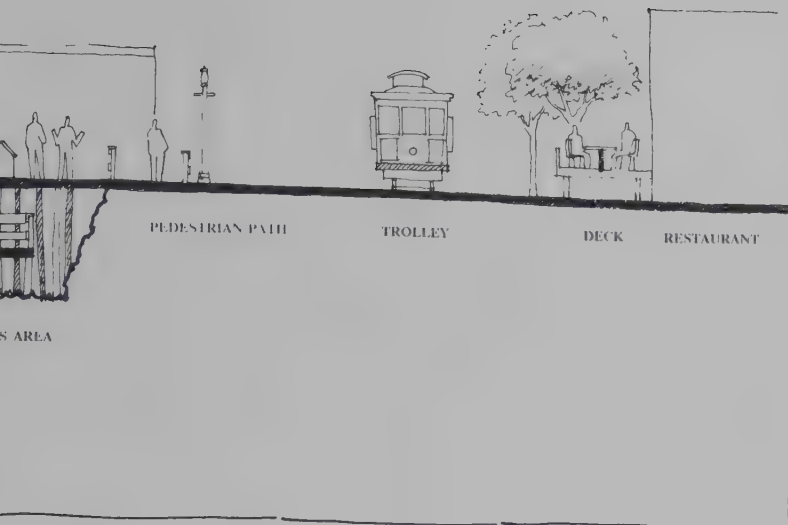
11TH/12TH STREET WATER ACCESS AREA SECTION

SCALE 1/8" = 1'-0"



BRIX MARITIME CORPORATION (KNAPPTON) BUILDING/DAY MOORAGE/PARKING SECTION

SCALE 1/8" = 1'-0"



ION

Figure 5.3

months.

- o A pedestrian crossing would be placed at the intersections of 14th Street/Marine Drive and 14th Street/Commercial to facilitate pedestrian circulation.
- o 14th Street from the river to Commercial would be enhanced with a new streetscape. Improvements would include lighting standards, street furniture, and landscaping.

✓ 14th to 15th Street

- o At the foot of 14th Street a new view deck could be located. To maximize views the deck could be located north of the existing office building and raised approximately 3 feet. A ramped deck would permit wheelchair access.
- o A 10 foot wide wood-planked pedestrian path would be developed north of the railroad right-of-way.
- o A pedestrian crossing would occur at the intersections of 15th Street/ Marine Drive and 15th Street/Commercial to facilitate pedestrian circulation.
- o The sidewalk and landscape buffer on the north side of Marine Drive south of the railroad right-of-way would be widened and enhanced.

15th to 16th Street

- o A 10-foot wide wood-planked pedestrian path would be developed north of the railroad right-of-way to approximately 50 feet east of Englund Marine. At

that point the path would then follow the shoreline. Due to the existing views offered and the proximity to the Columbia River Maritime Museum this area is envisioned as a popular location for walking and river watching. Lighting and street furniture would be incorporated to accommodate this anticipated use.

- o A floating net repair area would be located directly east of Englund Marine Supply. Views of net repair activity would be possible from the pedestrian path and seating areas located along the shoreline.
- o Angled parking for approximately 20 cars would be developed north of the right-of-way. The area would have two-way circulation to accommodate adjacent industrial/commercial uses.
- o Steps and additional decking would complement the existing structure at the People Places Park. To strengthen the visual connection between the park and the shoreline, wood-planks would extend north from the park structure to the pedestrian path. The landscape surrounding the park would be replanted with native type plantings that would reflect the history of the shoreline and would not burden the City's future operating budget with expensive maintenance.
- o A pedestrian crossing would be installed at the intersection of 16th Street and Commercial to

facilitate pedestrian circulation.

16th to 17th Street

- o A hard surfaced multi-level area would be developed west of the Maritime Museum plaza to provide a creative river access area for residents and visitors to the Museum.
- o The existing maritime dock would be extended west to allow for docking of vessels that would be used for interpretive and display purposes and for visiting tour boats.
- o A ten-foot wide wood plank pedestrian path would continue along the shoreline north of the right-of-way toward the museum. The existing parking area south of the path would be redeveloped as a 30-space angled lot with two-way circulation. Lighting and street furniture would be incorporated along this section of pathway.
- o The existing gravel parking lot would be developed into a 40-space paved parking area for museum visitors. A 10-foot landscape buffer along the north, south, and eastern edges would be incorporated to screen the parking area.
- o A large pre-cast concrete paving area would be installed to designate the entrance to the Maritime Museum property. A signal might be installed at the intersection of 17th Street and Marine Drive to allow easier ingress/egress to the museum.
- o 17th Street from the Maritime

Museum to the Heritage Museum would be developed with a new streetscape that would include lighting standards, street furniture, and landscaping to facilitate safe, clear pedestrian movement between the two museums.

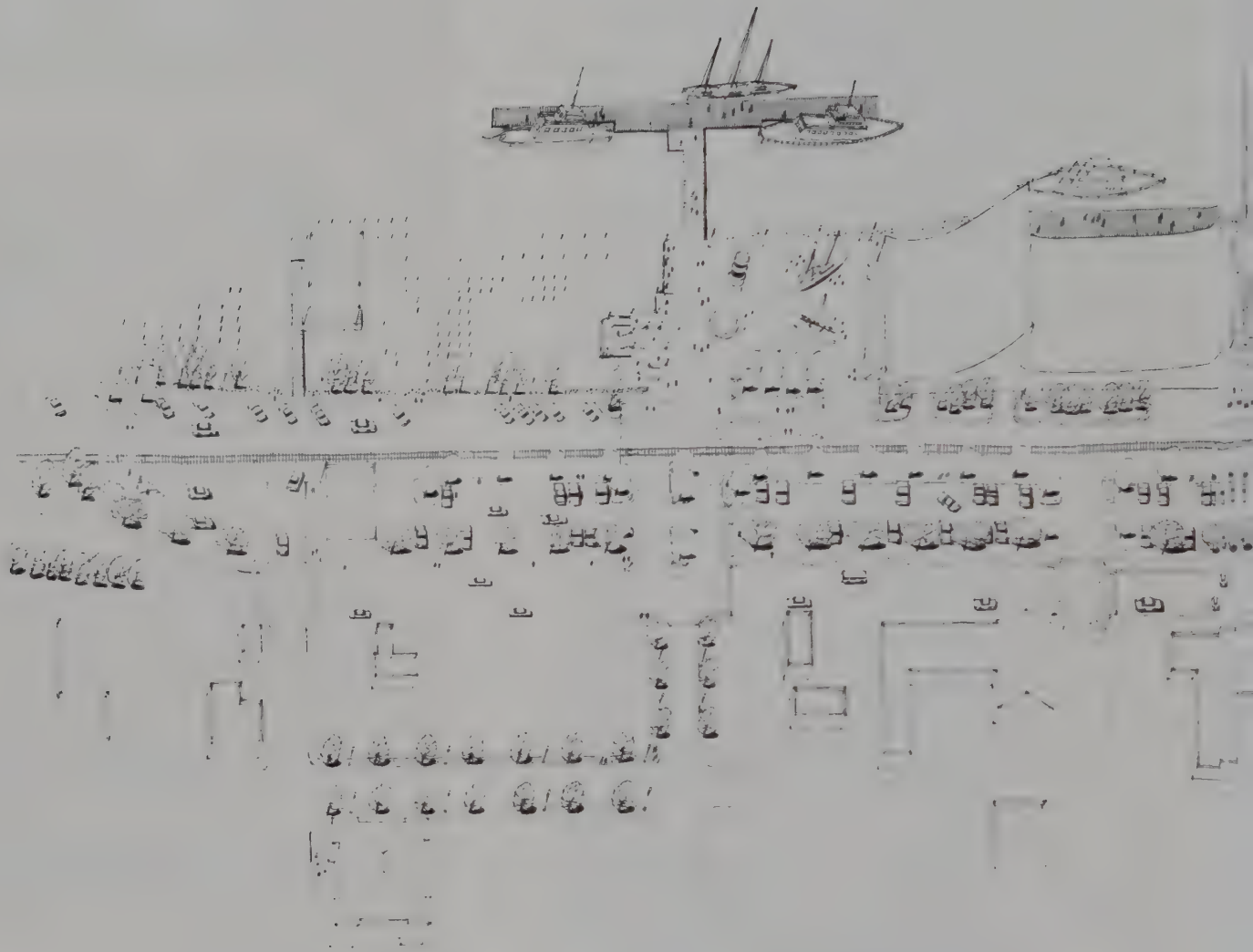
Maritime Museum

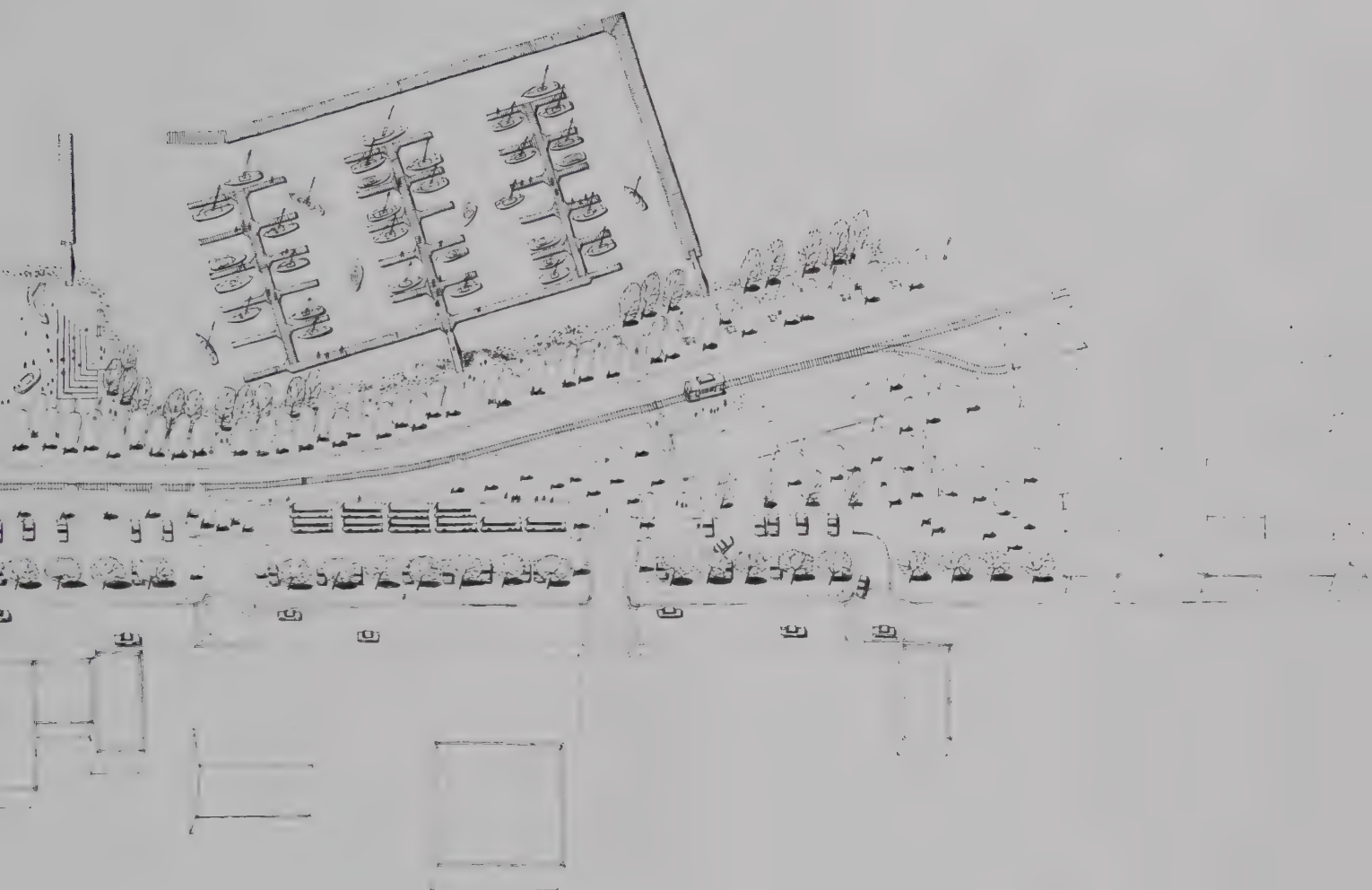
- o The plaza west of the museum would continue to be the main gateway to the museum and could be enhanced with visual displays, a tower, interpretive kiosks, an information center and additional plantings. The trolley could stop here for museum access.
- o A 4,500 square foot dock could be incorporated north of the museum to allow for additional boat displays and a central location for sight-seeing and interpretive tours of the Columbia River.
- o East of the museum, a secondary plaza could be developed with a small 150-seat amphitheater. The amphitheater could accommodate museum and community cultural events. A public restroom would be a part of this outdoor plaza.
- o A 50-slip day moorage area would be located east of the amphitheater, and a floating breakwater would be built in conjunction with the moorage to moderate summer winds from the northwest. The facility would serve as a downtown moorage area as well as accommodating museum day use.

- o The existing train station located on the site could eventually be redeveloped as the eastern trolley station and as a restaurant with outdoor seating.
- o A picnic area would be installed parallel to the shoreline.
- o A ten-foot wide concrete pathway would follow the shoreline through the site and would be developed with lighting and street furniture.
- o The graveled parking lot park south of the museum would be redeveloped as a 175-space parking area with provisions for recreational vehicle and bus parking.
- o A trolley repair yard would eventually be located east of the site.
- o If possible, the existing building south of the train station should be purchased and removed to open the view from Marine Drive and allow for additional parking.
- o The eastern waterfront gateway would be developed by the planting of street trees along Marine Drive, east of the museum.



Figure 5.4





COLUMBIA RIVER MARITIME MUSEUM AREA

SCALE 1" = 400'

Figure 5.4

ALTERNATIVE B

Alternative B is based on the concept that the City of Astoria will not obtain public access along the Burlington Northern right-of-way. The intent of this plan is to develop pockets along the waterfront that will encourage residents and visitors to view and experience the attributes that the downtown riverfront offers.

Pedestrian circulation through the project area is the main difference between this scheme and Alternative A. Though there is not formal pedestrian circulation within the railroad right-of-way, a path that promotes linkage between the 6th Street River Park and the Columbia River Maritime Museum, and access to street-end river viewing areas has been identified.

As with Alternative A, this plan was developed "on the premise that no single solution would revitalize the waterfront, and that a series of small improvements developed on a block-by-block basis would best satisfy the goals and desires of the Astoria community."

The following proposed improvements are common to both Alternative A and Alternative B:

- o Pedestrian crossings at Marine Drive and 7th, 9th, 10th, 11th, 12th, 15th, 16th, and 17th Streets,
- o Pedestrian crossings at Commercial and 8th, 9th, 10th, 11th, 12th, 14th, and 15th Streets,
- o Streetscape improvements along 8th, 10th, 11th, 12th, and from the Columbia River Maritime Museum to the Heritage Museum,
- o All future improvements identified in Alternative A between 6th and 9th Streets with the exception of the pedestrian path within the railroad right-of-way and the Trolley system,
- o View decks east and west of Pier 11,
- o Outdoor deck area north side of the Sanborn Block,
- o Outdoor deck north of building located between 11th and 12th Streets,
- o Multi-level wood-deck river access area and pile sculpture between 11th and 12th Streets,
- o Day moorage at the foot of 12th Street,
- o The Brix Maritime Corporation building redevelopment would be identical to what has been described in Alternative A except a roof would not be planned for the area.
- o Net repair area east of Englund Marine Supply,
- o Sidewalk and landscape buffer improvements on the north side of Marine drive between 14th and 15th Streets,
- o Pedestrian path, lighting and street furniture between 15th Street and the Columbia River Maritime Museum,

- o Reconfiguration of parking area between 15th Street and the Columbia River Maritime Museum,
- o People Place Park improvements,
- o River access area west of the Maritime Plaza,
- o Redevelopment of the Columbia River Maritime Museum parking area,
- o With the exception of the 17th Street Dock expansion and reconfiguration of the plaza entrance, all other Museum improvements would be identical to Alternative A, and
- o Eastern gateway to the waterfront area.

The following proposed improvements, unique to Alternative B, will be explained on a block-by-block basis. Further on in this report, areas of special significance will be explained in more detail.

5th to 6th Street

- o If the northern half of the block west of 6th Street were ever available for public purchase, a parking area could be developed within the site that would accommodate 17 cars. The two existing brick buildings and the metal frame structure have the potential of being redeveloped as a future commercial use. To buffer the parking area, a 10-foot wide landscaped area would be incorporated along the site's northern and eastern edge.
- o A pedestrian crossing would intersect the railroad right-of-way

at the end of Sixth Street to encourage pedestrian access to the Sixth Street River Park and the adjacent retail/office area.

6th to 8th Street

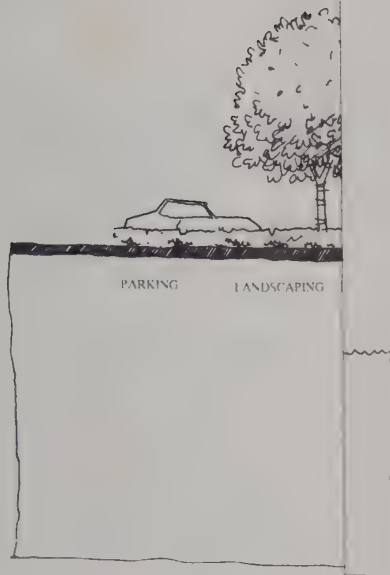
- o A pedestrian crossing would intersect the railroad right-of-way at the foot of 7th Street to provide access to the elevated view area. Wood-planking would be used as a walking surface at this and all other places where pedestrians cross the railroad-right-of-way.
- o Streetscape improvements would be incorporated along the north side of Marine Drive, and Sixth and Seventh Streets. Improvements could include sidewalk resurfacing, lighting, debris receptacles and plantings.

8th to 9th Street

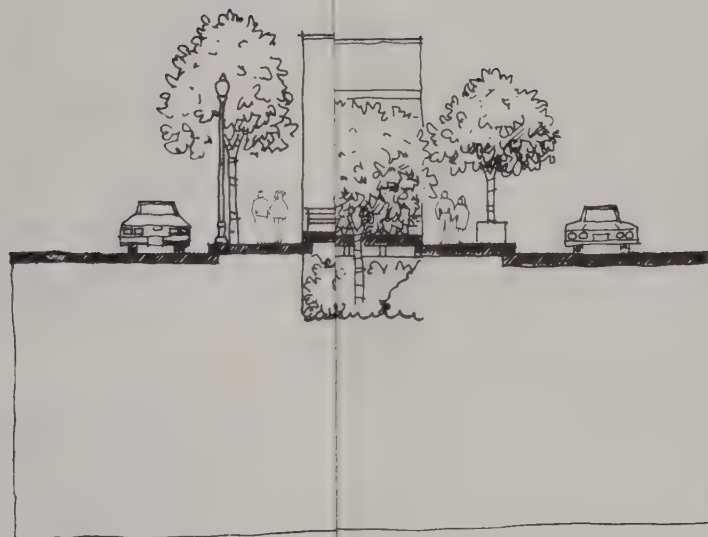
- o To allow access to the 8th Street terminus view area, a pedestrian crossing would intersect the railroad right-of-way at the foot of 8th Street.
- o Astor Street would be the primary circulation route through this section of the waterfront and, as in Alternative A, would be enhanced with streetscape improvements.

9th to 10th Street

- o To facilitate pedestrian circulation to the improved viewing area west of Pier 11, a pedestrian crossing would be implemented at the intersection of 10th Street and the railroad

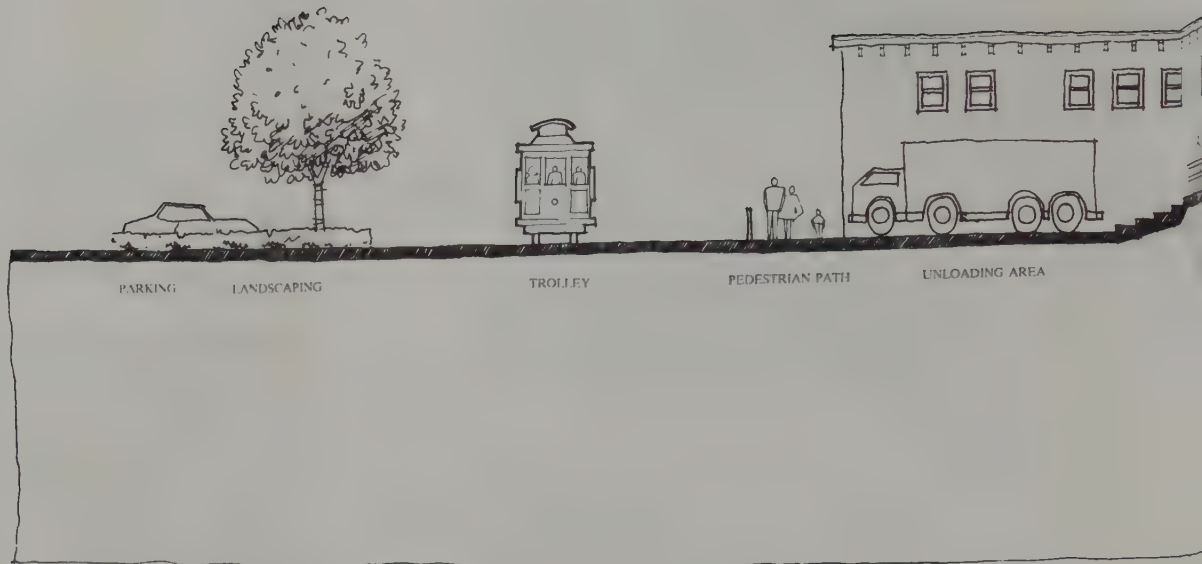


7TH STREET VIEW



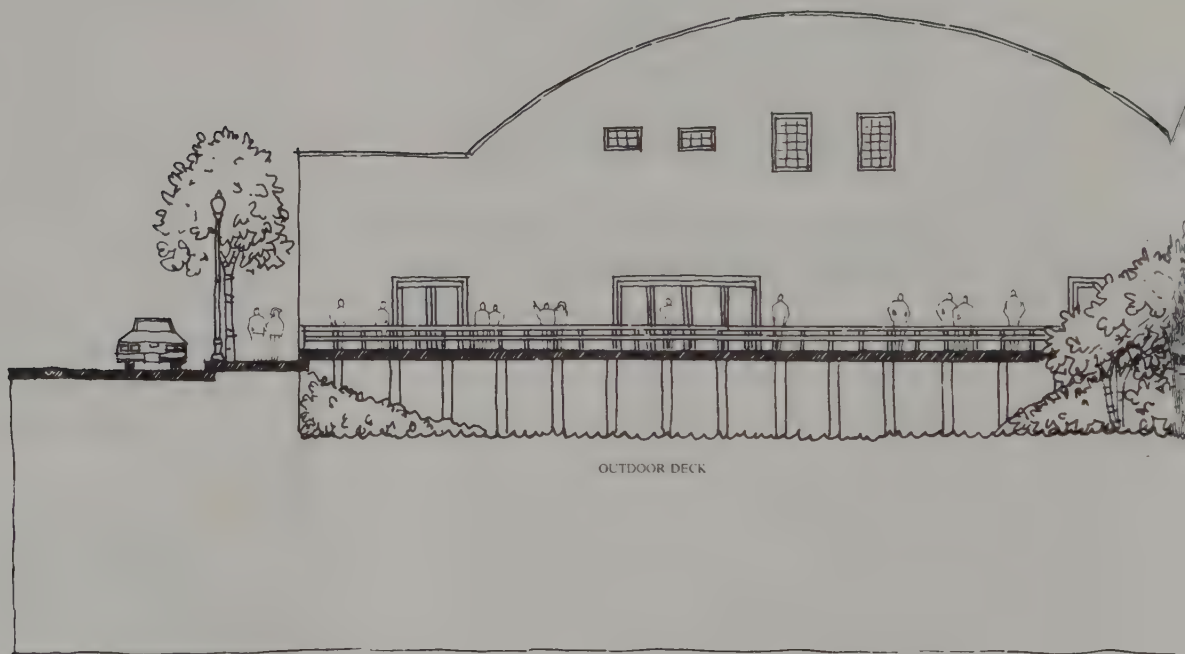
SANBORN BLOCK 9

Figure 5.5



7TH STREET VIEW AREA SECTION

SCALE 1/8" = 1'-0"



SANBORN BLOCK SECTION

SCALE 1/8" = 1'-0"

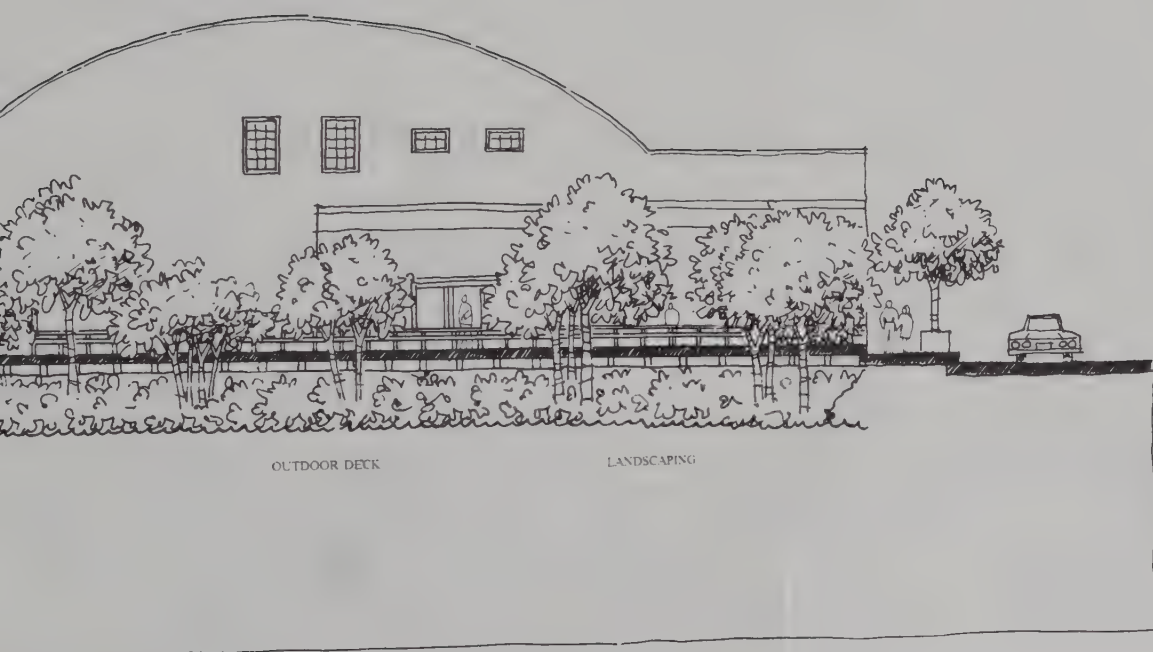
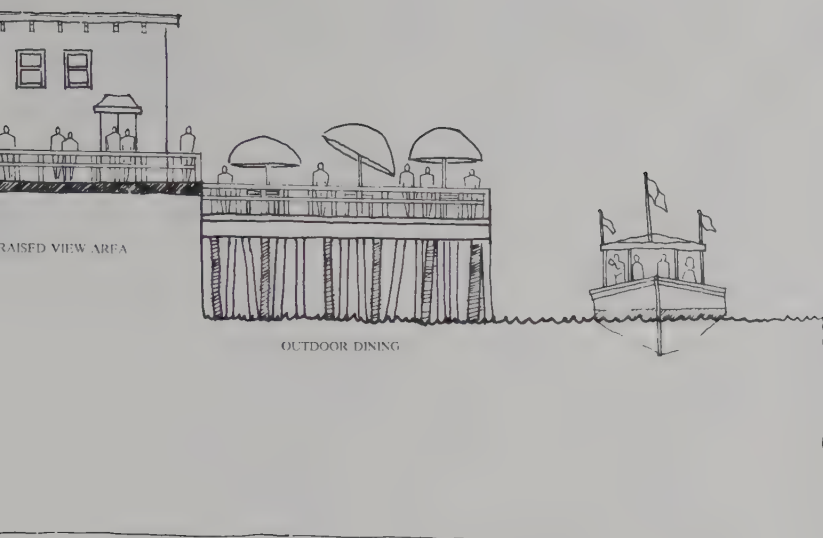


Figure 5.5

right-of-way.

10th to 11th Street

- o A pedestrian crossing would intersect the railroad right-of-way at the foot of 11th Street to encourage pedestrian circulation to the improved viewing area east of Pier 11.
- o Pedestrian circulation would flow from the intersection of Astor Street and 10th Street to the north side of Marine Drive. Streetscape improvements would be incorporated along the north and south sides of Marine Drive through this section. Improvements would include street furniture and lighting but not street trees.

11th to 12th Street

- o Pedestrian circulation would follow the north side of Marine Drive, and streetscape improvements would be incorporated along the north and south sides. Improvements would include street furniture and lighting but not street trees.
- o To allow access to the 12th Street day moorage, a pedestrian crossing would be implemented at the intersection of 12th Street and railroad right-of-way.

12th to 14th Street

- o Pedestrian circulation would follow the north side of Marine Drive. A streetscape would be developed on both sides through this section also. Improvements would include street furniture and

lighting but not street trees.

- o A pedestrian access area would intersect the right-of-way at the foot of 14th Street.

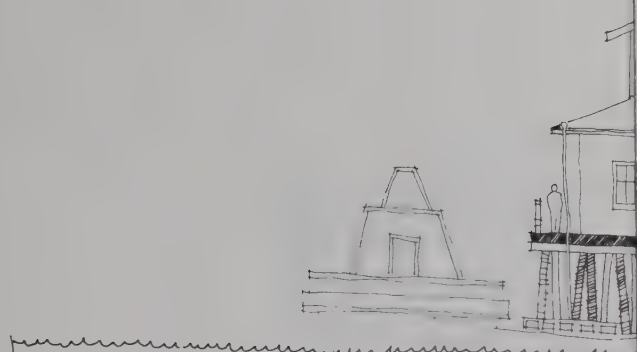
14th to 15th Street

- o A 10-foot wide wood-planked pedestrian path would be developed north of the right-of-way along with street furniture and lighting through this section.

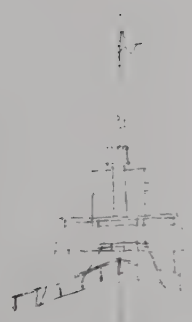
15th to 16th Street

- o A 10-foot wide wood-planked pedestrian path would be developed north of the right-of-way to approximately 50 feet east of Englund Marine Supply. At that point, the path would follow the shoreline. Lighting and street furniture would be incorporated along this section of path.
- o Angled parking for 20 cars would be developed north of the right-of-way with two-way circulation to accommodate adjacent industrial/commercial uses.

The remainder of the plan is identical to what has previously been described in Alternative A.



BRIX MARITIME CORPORATION (KNA



EXISTING MARITIME DOCK

REDEVELOPED PARKING

MARITIME MUSEUM WEST PLAZA SE

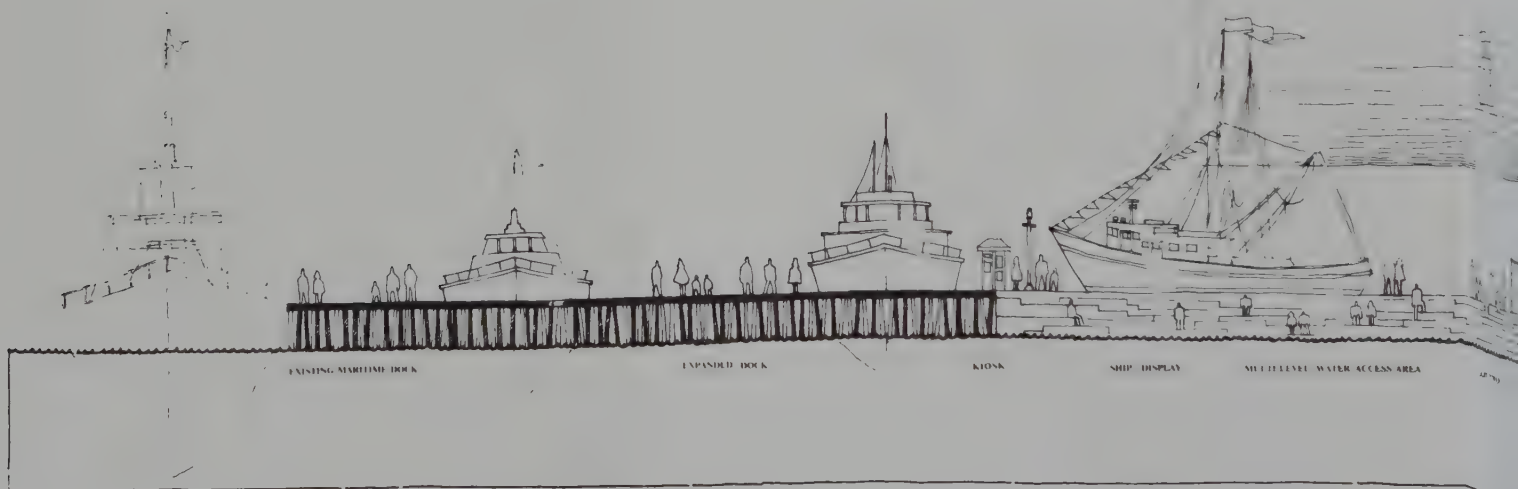
SCALE 1/8" = 1'-0"

Figure 5.6



BRIX MARITIME CORPORATION (KNAPPTON) BUILDING/DAY MOORAGE/PARKING SECTION

SCALE 1/8" = 1'-0"



MARITIME MUSEUM WEST PLAZA SECTION

SCALE 1/8" = 1'-0"

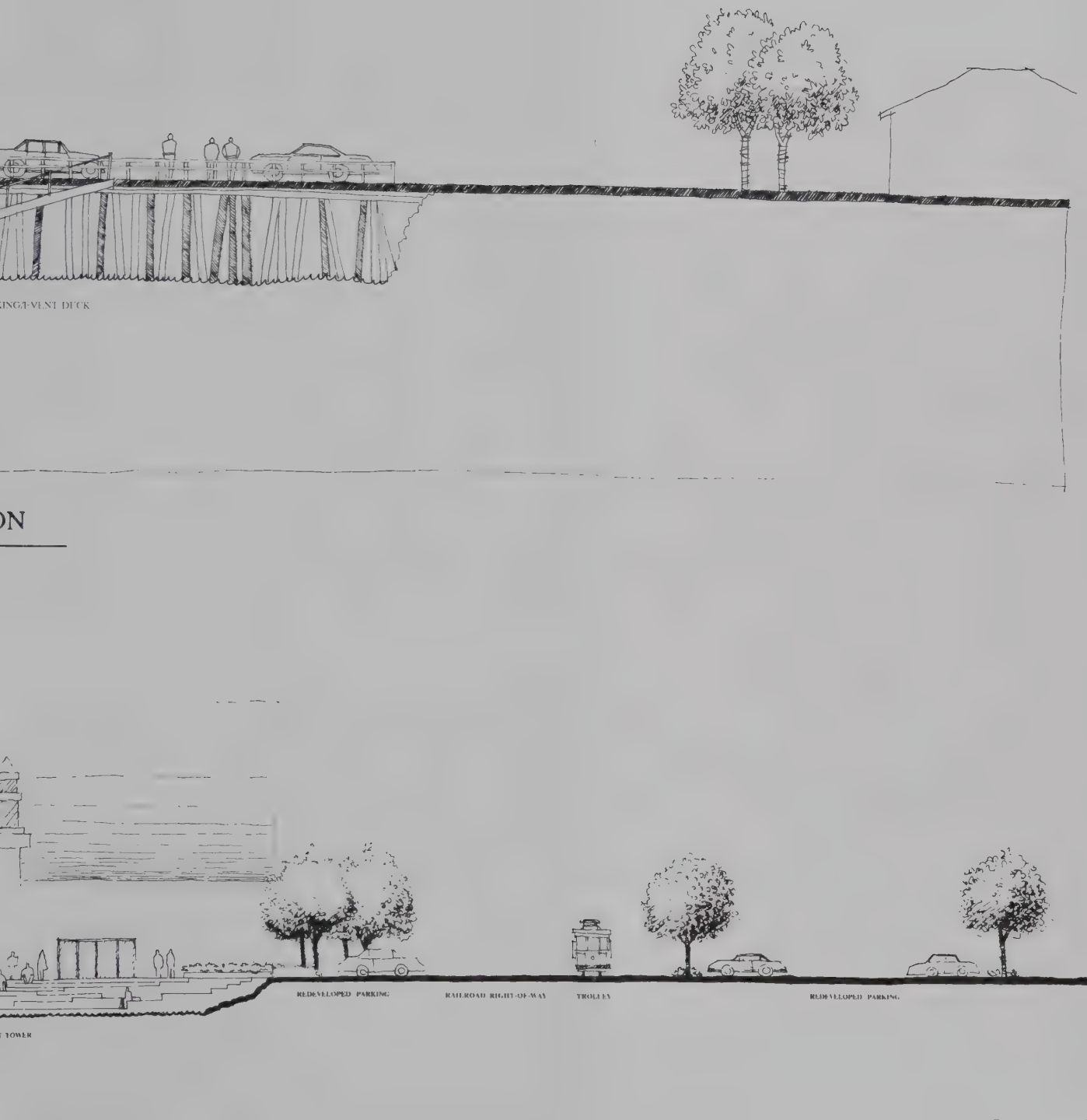
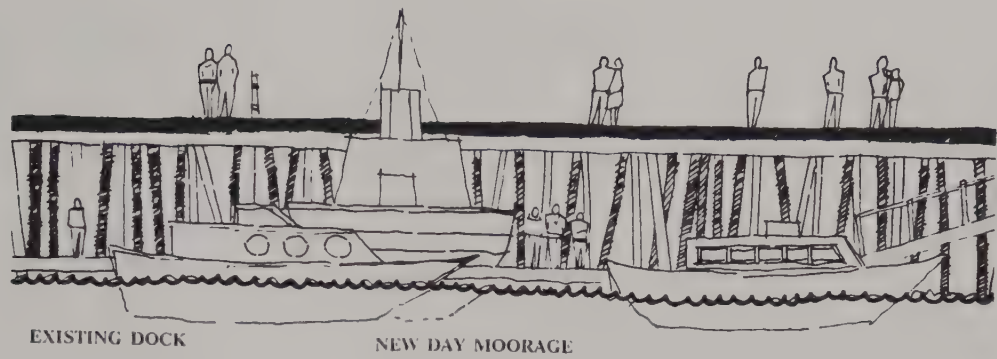


Figure 5.6



WATERFRONT PLACES AND FEATURES

WATERFRONT PLACES AND FEATURES

GENERAL

To give a more comprehensive understanding of the waterfront master plan, general design concepts are provided for the following waterfront places and features:

- o Streetscapes
- o Astor Street
- o Waterfront Pedestrian Path
- o View and Water Access Areas
- o Brix Maritime Corporation Building
- o Trolley

The intent of these descriptions is not to provide a finalized design solution but to provide a basis for subsequent more detailed design studies.

STREETSCAPES

Streetscape improvements are proposed for 5 distinctive situations within the waterfront and downtown area. These situations are:

- o the main pedestrian circulation path through the waterfront,
- o Astor Street,
- o the pedestrian circulation route between the Columbia River Maritime Museum and the Heritage Museum, and

- o 8th, 10th, 11th, 12th, and 14th Streets between the river and Commercial.

- o Entryways to the riverfront district

Streetscape improvements are intended to include:

- o street trees and plantings,
- o lighting,
- o sidewalks,
- o crosswalks,
- o street furniture (benches, trash receptacles, news paper dispensers, etc.)
- o building facades, and
- o signage/interpretive information

Improvements are encouraged to reflect the historical development of the City in regard to material and scale. It is essential that the theme, once established, be continuous throughout the riverfront and designated downtown streets. This continuity of materials and scale will aid in linking the waterfront with the downtown district.

Street Trees and Plantings

Recommended plantings consist primarily of trees.

Of first importance is the suitability of the selected tree variety to the Astoria climate. In selection of an appropriate street tree, the following should be taken into consideration:

- o hardiness and longevity,
- o enough variety of color and habit to aesthetically enhance the street,
- o mature branching and foliage should not obliterate storefronts,
- o mature canopy should provide shade and filtered light,
- o form should be complementary to the scale of existing buildings, and
- o columnar varieties should be avoided.

The following is a partial list of recommended tree varieties that will fulfill the intended purpose of a street tree:

- o Red Oak,
- o Sugar Maple,
- o Little Leaf Linden
- o Sycamore Maple



Proposed Street Trees

Where street construction permits in ground planting, street trees should be planted:

- o approximately 35 - 40 feet on center,
- o 25 feet from corners,
- o 25 feet from light standards, and
- o 20 feet from the edge of driveways.

Where in-ground planting is not possible, it is recommended that street trees not be used.

Tree grates that are square or rectangular in shape would conform to intended paving materials. The selected grating should be designed to accommodate the future growth of trees.

Planting pots and/or boxes, if used at all, should be located close to building facades. They should be located near other established features such as benches. Planting beds at street ends would hinder the streetscape continuity and are not recommended.

The use of native plants is suggested for areas that may require large plantings, People Place's Park at 16th Street for example. Plantings of native vegetation can offer seasonal color and diversity and aid in minimizing future maintenance costs.

Lighting

The development of appropriate lighting is critical to the aesthetic and historic quality of the waterfront and downtown

area. The intensity, quality, and color of light emitted must be considered along with safety and security. Careful thought needs to be given to the overall design and materials since the light fixtures will become a major design feature of the overall streetscape. The selected lighting standard should be used continuously through the waterfront and adjacent streets. Through use of single-style light fixtures, a continuous theme will be strengthened and aid in linking the various waterfront features together with the downtown.

Double bulb standards could be used in areas of special importance, the path along the railroad right-of-way for example, and single bulb standards along less critical streets, such as 8th Street.

It is recommended that a cast iron base/glass bulb lighting standard, similar in style and scale to what is presently in place along 10th Street, be used throughout the riverfront and downtown.

It is suggested that lighting standards be:

- o placed every 50 feet on center,
- o setback 25 feet from corners, and
- o setback 20 feet from driveway edges.

Sidewalks

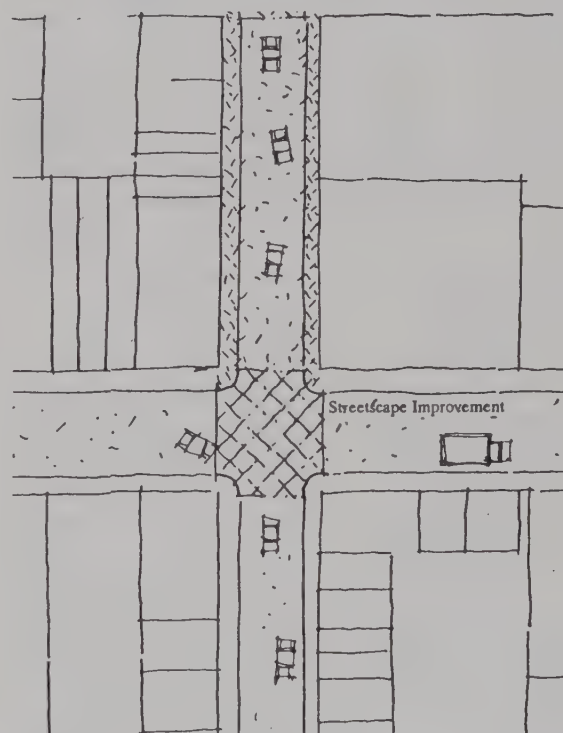
To further strengthen continuity, existing sidewalks could be resurfaced with a continuous paving material. Brick is a material that would provide a safe walking surface, durability, and a pleasing visual quality.

Walkways could be expanded at street ends to accommodate street crossings and enlarge the amount of available existing pedestrian space.

Crosswalks

Crosswalks should be clearly delineated as pedestrian areas to motorists. The material used should contrast with adjacent roadways and cover the entire intersection. Crosswalks designed this way would help identify the entire intersection as pedestrian space.

For the same reasons that brick would make an excellent sidewalk surface, brick would be appropriate as the crosswalk material.



Proposed Crosswalk Example

Street Furniture

Street furniture, consisting of benches, litter receptacles, drinking fountains, etc., will aid in providing an inviting atmosphere to residents and visitors.

As with lighting and paving surfaces, it is important that there be consistency in style and scale of street furniture throughout the study area and downtown. Adherence to the "working waterfront" and historic atmosphere are important criteria that must be considered along with form and durability.

Benches should be made primarily of wood, be simple in design, and approximately 6 feet in length. This could be an opportunity to utilize the skills of local artists and craftsman.

Benches should be grouped in areas of pedestrian congregation, at strategic locations such as impressive view points, and in front of a few building facades.

Bollards are useful in separating pedestrian and vehicular traffic areas and add interest to the street. They should be constructed of metal and be similar in design to proposed streetlight bases.

Building Facades

Many of the building facades in the downtown area are of architectural interest. The architectural integrity should be retained in any development.

The following guideline developed by the National Park Service for

restoration projects is not to be considered a specific list of do's and don'ts, but rather general thoughts to be taken into consideration at the time of rehabilitation:

- o Each structure should be evaluated as a separate entity and restored according to its respective style.
- o Effort should be made to provide a compatible use for buildings which will require minimum alteration to the building and its environment.
- o Rehabilitation work should not destroy the distinguishing qualities or character of the property.
- o Deteriorated architectural features should be repaired rather than replaced whenever possible.
- o Distinctive stylistic features or examples of skilled craftsmanship which characterize older structures should be treated with sensitivity.
- o All buildings should be recognized as products of their own time. Alterations to create earlier appearances should be discouraged.

Signage

Principal categories of signage include street and building identification signs, traffic control, directional and informational signs. There needs to be a consistency in scale, materials, proportions and location to reinforce the linkage between the downtown and riverfront areas.

Street signs may be mounted on separate standards and they should reflect the historic nature of city and waterfront.

Building identification signage could be, depending on the specific building, hung from the facade, applied to display windows, or possibly painted directly on masonry facades. Signage should not interfere with or obliterate building details, trim, or openings.

ASTOR STREET

As the western gateway to the waterfront district, Astor Street has the potential of becoming a pleasant, diversified walking experience for pedestrians. A small unique district that could support shops, galleries, restaurants, and water related uses would aid in giving the street a stronger sense of identity. The following long-range improvements are proposed:

- o sidewalk resurfacing,
- o lighting,
- o street furniture,
- o street resurfacing, and
- o additional retail.

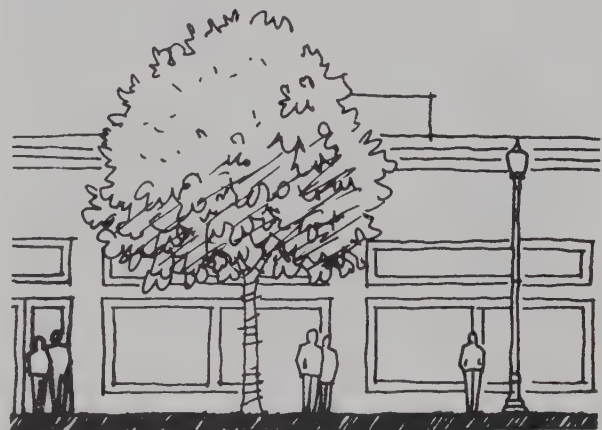
Presently, sidewalks along Astor Street are of standard width and are paved with concrete. By resurfacing the sidewalks on either side of the street and along 10th Street between the river and Marine Drive with brick, a defined pedestrian route could be formed.

Double bulb lighting standards consistent with those used in other parts

of the city would help give visual strength to the street.

Benches, litter receptacles, newspaper stands and other street furniture could be located in small groups at the street ends and places of interest.

Astor Street pavement could be resurfaced in concrete and scored with a pattern that could help bring it more in pedestrian scale. This is intended as a long-range future improvement.



RETAIL/COMMERCIAL

Proposed Commercial/Retail Development

The existing below-grade parking area at the southwest corner of Astor and 10th Street could provide a space for additional retail, restaurants, housing and parking. The complex could be laid out around a plaza that could provide a public space for various civic events. The architectural style of the future should be sensitive to the scale of the Sandborn Block, located along 10th Street.

WATERFRONT PEDESTRIAN PATH

Walking along the edge of the river is intended to be a special experience, not only because of the views that are offered, but because of the character and scale of the designated waterfront pedestrian path itself.

To strengthen the existing working waterfront character of the district, it is recommended that the path, within or adjacent to the railroad right-of-way, be constructed of wood planks. The path would be 10 feet wide and located within or adjacent to the north side of the railroad right-of-way throughout the plan area. Wood railing, 36 inches in height, would be incorporated along the north and south edges of the path for separation from the river and the potential future trolley.

Double bulb lighting, compatible with other streetscape lighting, would be located approximately every 50 feet along the entire area. Wood benches would be located at points offering exceptional views of the river. When plaza improvements are installed at the Maritime Museum outdoor lighting should be compatible with what is installed along the pedestrian path. Plantings would be installed along the north side of adjacent parking areas, west of the Maritime Museum. Native plantings should be used for maintenance and aesthetic reasons.

VIEW AND OUTDOOR DECK AREAS

Views of the Columbia River and the activity upon it makes the waterfront a unique and special place. Though other public view areas exist at the Sixth Street River Park and Maritime Museum Plaza, there is an opportunity to further enhance the aesthetic qualities of the Astoria riverfront.

Even with the working waterfront character, there is no place within the downtown riverfront to actually touch the water. In response to this need, two multi-leveled areas are proposed where visitors could walk down to the river from the shoreline.

Based upon access and quality of scenic view, view areas are proposed at the following locations:

- o the foot of 7th Street,
- o immediately west of Ocean Foods Seafood Processing plant,
- o at the foot of 10th and 11th Streets, east and west of the Pier 11 Restaurant,
- o along the shoreline between 11th, and 12th Streets,
- o at the foot of 14th Street, and
- o west of the Columbia River Maritime Museum.

To further increase the amount of seable outdoor space within the waterfront, new and expanded decking areas are proposed:

- o along the north side of the Sanborn Block,
- o along the north side of the existing building between 11th and 12th Streets, Railroad right-of-way extends to building wall
- o People Places Park.

To reinforce the continuity of the waterfront and promote the "working waterfront character", all view areas, with the exception of the water access area immediately west of the Maritime Museum, should be constructed of wood. A cohesiveness should exist in the materials and scale of railings, benches, steps, kiosks, and covered shelters.

Foot of 7th Street View Area

A 400-square-foot view area is intended for this location. Due to conflict between truck loading/unloading and pedestrian use, it is recommended that the actual viewing area be elevated a few feet. A kiosk could be placed here that would provide information on the history of seafood processing in Astoria. To provide wheel chair access, a ramp should be incorporated.

West of Ocean Foods

A six-foot-wide catwalk and 100 square foot pile-supported deck would allow seafood processing activities to be watched without interference.

Foot of 10th and 11th Streets, east and west of Pier 11

Two existing pile-supported decks presently exist east and west of the Pier

11 Restaurant. Currently both decks are used primarily for restaurant parking. To provide additional public outdoor space and accommodate the need for restaurant parking, both deck areas could be enlarged.

If the west deck was expanded by approximately 2,500 feet, the area could accommodate a 600 square foot open-sided metal roofed shelter, outdoor eating space, and viewing benches. A kiosk that provides information about the vessels that travel the river would enrich the space. With the exception of 3 designated handicapped parking spaces, the view area is intended to be automobile free.

To provide for restaurant parking, the east deck could be enlarged by approximately 4,000 square feet. This expansion would allow 18 parking spaces. The north 10 feet of the deck area could be designated for pedestrian use. From this area visitors could view the river and the activities of the adjacent fish receiving facility.

Shoreline between 11th and 12th Streets

This deck area is intended to be a series of platforms interconnected by steps that descend from the shoreline to the river. Railings, would be incorporated to provide safety from strong currents and significant ship wakes. Designed in this manner, visitors would be able to have contact with the river regardless of the tide level. To provide a place for river watching during foul weather, a small 200 square foot shelter is intended to

be incorporated here. Benches would be located within the shelter. Through development of an on-grade brick pedestrian walkway, the linkage between this area and the proposed outdoor deck directly south would be strengthened.

Foot of 14th Street

A 400 square foot deck area, located approximately 80 feet north of the shoreline could provide excellent views of the tug boat and river/bar pilot activity immediately to the west and dramatic views to the east. To enhance view opportunities, the deck area could be raised and wheel-chair access provided by a ramped walkway between the shoreline and the deck.

An interpretive kiosk, that described the activities and importance of the tug boat activity and the river/bar pilots as well as the ferry that formerly occupied the site would be educational for visitors.

Sanborn Block (located between 10th and 11th Street south of the Burlington Northern Railroad right-of-way)

The intent of this 2,500 square foot, single-level deck is to provide an additional space for outdoor eating, display, and to open the building towards the river. Benches, litter receptacles, lighting, and railings would be similar in style to other street furniture in the waterfront.

Maki/Riviera Block (located between 11th and 12th Street, Railroad right-of-way extends to building wall.)

This proposed 3,500 square foot deck would be similar in style and function to the Sanborn Block deck area.

People Place's Park

To strengthen the linkage between the existing park and the shore line, the north side of the deck could be opened up with wide steps that would encourage access to the walkway along the shore line. A brick on-grade crosswalk would aid pedestrian circulation between the deck and the proposed walkway.

Landscaping around the deck area could be enhanced with planting of native vegetation. It is recommended that any trees planted be deciduous so that views will not be impaired from Marine Drive, west of the Columbia River Maritime Museum.

A future long-range opportunity is the development of a concrete multi-level area that would encourage access to the river. Comprised of a series of platforms, interconnected landings, ramps and steps, this area could provide a safe, fun and informative route to the river. Primary access is intended to be from the Maritime Museum Plaza.

BRIX MARITIME CORPORATION BUILDING

The Brix Maritime Corporation complex, located between 12th and 14th Streets, is comprised of 2 over water structures which are linked by a dock. The Astoria Marine Supply building

forms the western edge of the alcove which offers calm water and interesting views of the river/bar pilot and tug boat activity.

The second building, approximately 12,500 square feet in size, is built on piles directly north and adjacent to the Burlington Northern Railroad right-of-way. The building is in need of extensive repairs and the opportunity exists to create a multi-purpose parking/special event space in this location. Given that the existing piles are structurally sound, a wood deck could be constructed that would accommodate up to 40 parking spaces.

The deck area is intended to be 1 level with railing that is consistent with other view areas. Two kiosks could be placed here that could provide information about river pilots, tug operations and the river ecosystem.

It is recommended that a tin roof with clerestory windows be incorporated to offer weather protection for weekend markets/festivals and other public events. The overall style and material of the roof should be consistent with other proposed shelters along the waterfront.

A 400 square foot restroom incorporated within this shelter is proposed for this location. The architecture of the restroom should be compatible with proposed waterfront improvements.

The protective setting of this alcove makes this area an ideal location for a

downtown summer moorage area. A six-slip moorage space and floating dock could be developed at the foot of 12th Street. This location would afford easy access to downtown and other waterfront places.

TROLLEY

The proposed trolley/rubber tired vehicle system along the waterfront is intended to be viewed as a long-range future improvement. Upon implementation, the system would operate between the Columbia River Maritime Museum and the Sixth Street River Park. Secondary stops would be located at the foot of 10th Street (Pier 11 Bar), between 12th and 14th Streets (Brix Maritime Corporation Building complex) and at the entrance to Columbia River Maritime Museum.

Two trolley stations are suggested for ticket purchase and boarding. These stations are recommended to be located at the eastern and western anchors of the waterfront.

The eastern trolley station could occupy the former Burlington Northern Train Station, east of the museum. This facility would accommodate the needs of the trolley as well as provide a location for an informal restaurant and a covered outdoor eating area.

A raised landing for entering and exiting the trolley would be constructed along the north side of the building. The landing would be paved in brick to

reflect the architecture of the former train station.

Lighting, benches, and railing incorporated here would be consistent with other waterfront places.

A pre-fab metal building, large enough to store 2 trolley vehicles and perform maintenance functions, would be located east of the train station.

If the privately owned brick building, at the foot of 6th Street and adjacent to the Burlington Northern Railroad right-of-way, was ever to become available and was purchased by the City it could serve as the western trolley station.

The existing restrooms within the building could be refurbished and a raised brick landing could be developed for to entrance and exit from the trolley. Lighting, benches and railing used here would be similar to what is used in other waterfront view areas and streetscapes.

Secondary trolley stops would be consistent in size and material selection. It is recommended that an uncovered, raised 200 square foot landing, paved in brick, be incorporated at each of these places. Lighting, street furniture and railings should also be consistent with the overall waterfront development plan.



PHASING

PHASING

The master plan that represents the final planning effort is conceptual in nature, presenting a set of goals, guidelines and recommendations for implementing specific improvements. Before the plan can be implemented, a more detailed level of study is required to define the scope and impacts of these specific improvements.

Phasing of priority areas is directly related to funding. This means that phasing will be necessary to implement the entire master Plan program. Therefore, developing criteria for establishing priorities is a critical part of the planning and funding process. Criteria that can be used in formulating priorities for the master plan are as follows:

- o Areas that would receive the highest use and demand would receive a high priority.
- o Phasing levels would be within funding range.
- o Phasing scheme must lead to development continuity in future phasing programs.

As a result of the planning team's twelve-month effort regarding the future of the waterfront, the following phasing of priority areas is recommended:

Priority 1:

Pedestrian Pathway

A waterfront pathway would be the spine of the waterfront and link the plan.

Priority 2:

View Area and Outdoor Deck Areas

View areas developed at street ends would provide an opportunity for river viewing and help visually link downtown with the waterfront. Outdoor decks would be built in association with Sanborn and Maki Riviera blocks.

Priority 3:

Streetscapes

Sidewalk resurfacing, light standards, street trees and street furniture would promote pedestrian circulation between downtown and the waterfront.

Parking

Increased parking space is necessary for present and future waterfront use.

Priority 4:

Restrooms

Public restrooms at identified locations would facilitate the useability of the area and reduce the burden presently placed on private business operations.

Landscaping

Buffering around parking areas and at entry points would improve the aesthetic appearance of the waterfront.

Priority 5:

Columbia River Maritime Museum

As the eastern anchor of the waterfront area, improvements in the form of increased parking, plaza enhancement, and landscaping would enhance the overall downtown riverfront.

Brix Maritime Corporation (Knappton) Building

Improved deck area and roof would provide additional parking for waterfront users and a much needed protective open area for occasional festivals, art fairs and a possible weekend market.

Priority 6:

Astor Street

Streetscape improvements would enrich the existing character of the area and promote visitors to stroll from one end of Astor Street to the other.

10th & Astor Street Commercial/Retail

A future opportunity exists to develop additional retail/office space on the existing parking area at 10th & Astor Street.

Priority 7:

Trolley

Eventual implementation of a trolley or rubber-tired vehicle from 6th Street River Park to the Columbia River Maritime Museum would serve to link the waterfront area.

Cost Analysis

ASTORIA WATERFRONT
 PLAN A
 PUBLICLY FUNDED PROJECTS

CONSTRUCTION ITEM	CONST UNIT	UNIT COST	# OF UNITS	TOTAL COST	SUMMATION
1. Utility Improvements (To include sewer, water & electrical servise)	To Be Determined				
2. Pedestrian Paths					
A. Acquisition of Northern 10' of B-N-R Right-Of-Way 10' Plank	To Be Determined				
B. Path, Railing, etc. (10' Wide, Wood Plank)	SF	20.00	34,550	\$691,000	
C. Lighting	EA	2,000.00	50	\$100,000	
D. Street Furniture	LUMP			\$24,000	
3. Special Pavement Areas					
A. Maki/Riviera Block (Wood Plank)	SF	12.00	6,400	\$76,800	
B. People Places Park Pedestrian Crossing (Wood Plank)	SF	12.00	2,600	\$31,200	
C. B.N.R. Pedestrian Crossing (Wood Plank)	SF	12.00	9,200	\$110,400	
4. Trolly Improvements					
A. West Trolly Station - Redevelop existing structure	SF	50.00	1,500	\$75,000	
- Pavement (Brick, Concrete Base)	SF	\$8.5	1,100	\$75,000	
B. Brix Maritime Corp. - Trolly Stop (Brick, Concrete Base Platform, Railing)	SF	20.00	1,000	\$20,000	
C. Pier II Restaurant Trolly Stop (Brick, Concrete Base Platform, Railing)	SF	20.00	1,000	\$20,000	
D. Vehicle	EA	100,000.00	3	\$300,000	
5. View Areas					
A. Foot of 7th Street (Wood plank, Rail, Steps, Pilings)	SF	40.00	400	\$16,000	
B. Foot of 8th Street	SF	40.00	500	\$20,000	
* C. Foot of 10th Street (Wood Plank, Pilings, Steps, Railing)	SF	40.00	2,500	\$100,000	
- Shelter	SF	20.00	500	\$10,000	
* D. Foot of 11th Street (Wood plank, pilings, steps, railing)	SF	40.00	1,600	\$64,000	
* E. 11th to 12th Street - Water Access Area (Wood Plank, Pilings, Steps, Railing)	SF	40.00	2,500	\$100,000	
- Shelter	SF	30.00	200	\$6,000	

PUBLICLY/PRIVATELY FUNDED PROJECTS

CONSTRUCTION ITEM	CONST UNIT	UNIT COST	# OF UNITS	TOTAL COST	SUMMATION
* F. Foot of 14th Street (Wood Plank, Railing, Pilings)	SF	40.00	1,250	\$50,000	
* G. 17th Street Water Access Area (Pilings, Concrete, Railing, Etc.)	SF	40.00	3,000	\$120,000	
6. Outdoor Deck Areas					
* A. Sanborn Block (Wood Deck Rail)	SF	12.00	2,500	\$30,000	
* B. Maki Riviera Block (Wood Deck Rail)	SF	12.00	3,500	\$42,000	
C. People Places Park (Additional Deck, Steps, Railing)	SF	12.00	400	\$4,800	
7. Marina Improvements					
A. 12th Street Marina (6 Slips, Dock)	EA	10,000.00	6	\$60,000	
8. Commercial Development					
* A. Brix Maritime Corp. Bldg. - Redevelopment of Existing Dock (Deck, Railing, Etc.)	SF	15.00	13,650	\$204,750	
- Roof	SF	14.00	12,500	\$175,000	
9. Restrooms	SF	175.00	300	\$52,500	
A. 14th Street (Wood Construction)					
10. Parking					
To include 6" structural base with 2 1/2 Class C asphalt, 6"x16" poured in place curbing.					
A. 5th/6th Street Lot					
- Stalls	EA	1,500.00	31	\$46,500	
- Lighting Standards	EA	2,000.00	4	\$8,000	
- Landscaping	LUMP			\$6,000	
B. 6th/7th Street Lot					
- Stalls	EA	1,500.00	80	\$120,000	
- Lighting Standards	EA	2,000.00	12	\$24,000	
- Landscaping	LUMP	7,500.00		\$7,500	
C. Bornstein's					
- Stalls	EA	1,500.00	24	\$36,000	
- Lighting Standards	EA	2,000.00	4	\$8,000	
- Landscaping	LUMP	3,800.00		\$3,800	
D. 15th-17th Street					
- Stalls	EA	3,000.00	52	\$156,000	
- Landscaping	LUMP			\$9,500	

Cost Analysis

CONSTRUCTION ITEM	CONST UNIT	UNIT COST	# OF UNITS	TOTAL COST	SUMMATION
<hr/>					
11. Astor/10th Street Redevelopment					
A. Street Resurfacing	LF	300.00	680	\$204,000	
B. Sidewalk Resurfacing	SF	8.50	13,000	\$110,500	
C. Lighting Standard	EA	2,000.00	12	\$24,000	
D. Street Furniture	LUMP			\$14,000	
E. Street Trees	EA	1,000.00	15	\$15,000	
12. Streetscapes					
* A. Crosswalks (Arterial Pedestrian Crossing)	SF	8.50	77,300	\$657,050	
* B. 8th Street					
- Sidewalk Resurfacing	SF	8.50	10,000	\$85,000	
- Light Standards	EA	2,000.00	22	\$44,000	
- Street Trees	EA	1,000.00	22	\$22,000	
- Street Furniture	LUMP			\$15,000	
* C. 10th Street					
- Sidewalk Resurfacing	SF	8.50	8,000	\$68,000	
* D. 12th Street					
- Sidewalk Resurfacing	SF	8.50	5,200	\$44,200	
- Light Standard	EA	2,000.00	12	\$24,000	
- Street Trees	EA	1,000.00	12	\$12,000	
- Street Furniture	LUMP			\$12,000	
* E. 14th Street					
- Sidewalk Resurfacing	SF	8.50	4,000	\$34,000	
- Light Standard	EA	2,000.00	8	\$16,000	
- Street Trees	EA	1,000.00	8	\$8,000	
- Street Furniture	LUMP			\$10,000	
* F. 17th Street to Heritage Museum					
- Sidewalk Resurfacing	SF	8.50	9,000	\$76,500	
- Light Standard	EA	2,000.00	20	\$40,000	
- Street Trees	EA	1,000.00	20	\$20,000	
- Street Furniture	LUMP			\$13,000	
* G. 14th - 15th Street					
- Sidewalk Resurfacing	SF	8.50	3,500	\$29,750	
- Light Standard	EA	2,000.00	6	\$12,000	
- Street Trees	EA	1,000.00	6	\$6,000	
13. Landscaping					
- People Place Park	LUMP			12,000.00	
- Eastern Gateway	LUMP			20,000.00	
- Western Gateway	LUMP			10,000.00	
14. Signalization	LUMP			\$100,000	
17th & Marine Drive					
SUB-TOTAL: PUBLIC				\$2,169,000	
PRIVATE/PUBLIC				\$2,517,750	
GRAND TOTAL:				\$4,686,750	
				=====	

~~\$68,000~~

12-Jun-90

Cost Analysis

ASTORIA WATERFRONT

PLAN A

PRIVATELY FUNDED PROJECTS

CONSTRUCTION ITEM	CONST UNIT	UNIT COST	# OF UNITS	TOTAL COST	SUMMATION
A. COLUMBIA RIVER MARITIME MUSEUM					
1. Pedestrian Paths					
A. Pathway (10' Wide Concrete)	SF	\$2.50	9,800	\$24,500.00	
B. Lighting	EA	\$2,000.00	15	\$30,000.00	
C. Street Furniture	LUMP			\$18,000.00	
2. Special Pavement Area					
A. Main Entrance Area (Brick Concrete Base)	SF	\$8.50	31,775	\$270,087.50	
3. Trolly Improvements					
A. East Trolly Station					
- Redevelop Former Train Station	SF	\$50.00	5,175	\$258,750.00	
- Brick Concrete Base	SF	\$8.50	2,000	\$17,000.00	
B. Trolly Repair Building (Pre-fab Metal)	SF	\$40.00	2,400	\$96,000.00	
C. Museum Trolly Stop (Brick, Concrete Base, Railing, etc.)	SF	\$20.00	1,200	\$24,000.00	
4. Marinas					
A. Museum Marine					
- (50 slips/Dock)	EA	\$10,000.00	50	\$500,000.00	
- Floating Breadwater	SF	\$150.00	680	\$102,000.00	
5. Parking					
- Stalls	EA	\$1,500.00	200	\$300,000.00	
- Light Standards	EA	\$2,000.00	60	\$120,000.00	
- Paths (6' Wide Concrete)	SF	\$2.50	3,000	\$7,500.00	
6. Maritime Dock					
A. Maritime Dock Expansion	SF	\$40.00	2,400	\$96,000.00	
B. New Maritime Dock	SF	\$40.00	4,500	\$180,000.00	
7. West Plaza					
A. Tower	To Be Determined				
B. Interpretive Ship	To Be Determined				
C. Lighting Standard	EA	\$2,000.00	6	\$12,000.00	
D. Kiosks	EA	\$2,000.00	6	\$12,000.00	

Cost Analysis

CONSTRUCTION ITEM	CONST UNIT	UNIT COST	# OF UNITS	TOTAL COST	SUMMATION
<hr/>					
8. East Plaza					
A. Paving	SF	\$3.00	12,500	\$37,500.00	
B. Amphitheatre	SF	\$20.00	5,000	\$100,000.00	
C. Lighting Standard	EA	\$2,000.00	8	\$16,000.00	
D. Kiosks	EA	\$2,000.00	4	\$8,000.00	
9. Picnic Area (Tables, Concrete, Pad)	EA	\$1,000.00	10	\$10,000.00	
10. Signalization 17th Street & Marine Drive	EA	\$100,000.00	1	\$100,000.00	
11. Landscaping	LUMP			\$46,000.00	
12. Restroom	SF	\$175.00	300	\$52,500.00	

		SUB-TOTAL:		\$2,437,837.50	
 B. VIEW DECKS					
1. 6th Street Dining Deck (Wood Plank, Pilings)	SF	\$40.00	1,500	\$60,000.00	
 C. COMMERCIAL DEVELOPMENT					
1. Astor/10th Street					
- Redevelopment	SF	\$60.00	26,000	\$1,560,000.00	
- Retail/Commercial	SF	\$3.50	12,600	\$44,100.00	
- Public Plaza	SF	\$60.00	750	\$45,000.00	
2. 6th Street Commercial Building	SF	\$175.00	300	\$52,500.00	

		SUB TOTAL:		\$1,701,600.00	
		GRAND TOTAL:		\$4,199,437.50	
				=====	

ASTORIA WATERFRONT
 PLAN B
 PUBLICLY FUNDED PROJECTS

CONSTRUCTION ITEM	CONST UNIT	UNIT COST	# OF UNITS	TOTAL COST	SUMMATION
1. Utility Improvements (To include sewer, water & electrical servise)	To Be Determined				
2. Pedestrian Paths (Brick Concrete Base)	SF	\$8.50	31,000	\$263,500.00	
- Lighting	EA	\$2,000.00	60	\$120,000.00	
- Street Furniture	LUMP			\$24,000.00	
3. Special Pavement Areas					
A. Maki/Riviera Block (Wood Plank)	SF	\$12.00	6,400	\$76,800.00	
B. People Places Park Pedestrian Crossing (Wood Plank)	SF	\$12.00	2,600	\$31,200.00	
C. B.N.R. Pedestrian Crossing (Wood Plank)	SF	\$12.00	9,200	\$110,400.00	
4. View Areas					
A. Foot of 7th Street (Wood plank, Rail, Steps, Pilings)	SF	\$40.00	400	\$16,000.00	
* B. Foot of 8th Street	SF	\$40.00	500	\$20,000.00	
* C. Foot of 10th Street (Wood Plank, Pilings, Steps, Railing)	SF	\$40.00	2,500	\$100,000.00	
- Shelter	SF	\$20.00	500	\$10,000.00	
* D. Foot of 11th Street (Wood plank, pilings, steps, railing)	SF	\$40.00	1,600	\$64,000.00	
* E. 11th to 12th Street - Water Access Area (Wood Plank, Pilings, Steps, Railing)	SF	\$40.00	2,500	\$100,000.00	
- Shelter	SF	\$30.00	200	\$6,000.00	
* F. Foot of 14th Street (Wood Plank, Railing, Pilings)	SF	\$40.00	1,250	\$50,000.00	
* G. 17th Street Water Access Area (Pilings, Concrete, Railing, Etc.)	SF	\$40.00	3,000	\$120,000.00	
5. Outdoor Deck Areas					
* A. Sanborn Block (Wood Deck Rail)	SF	\$12.00	2,500	\$30,000.00	
* B. Maki Riviera Block (Wood Deck Rail)	SF	\$12.00	3,500	\$42,000.00	
C. People Places Park (Additional Deck, Steps, Railing)	SF	\$12.00	400	\$4,800.00	

Cost Analysis

CONSTRUCTION ITEM	CONST UNIT	UNIT COST	# OF UNITS	TOTAL COST	SUMMATION
6. Marina Improvements					
A. 6th Street Commercial Building	SF	\$60.00	\$750.00	\$45,000.00	
7. Commercial Development					
* A. Brix Maritime Corp. Bldg. - Redevelopment of Existing Dock	SF	\$10.00	13,650	\$136,500.00	
8. Restrooms					
A. 14th Street (Wood Construction)	SF	\$175.00	300	\$52,500.00	
9. Parking					
To include 6" structural base with 2 1/2 Class C asphalt, 6"x16" poured in place curbing.					
A. 5th/6th Street Lot					
- Stalls	EA	\$1,500.00	17	\$25,500.00	
- Lighting Standards	EA	\$2,000.00	4	\$8,000.00	
- Landscaping	LUMP			\$6,000.00	
B. 6th/7th Street Lot					
- Stalls	EA	\$1,500.00	80	\$120,000.00	
- Lighting Standards	EA	\$2,000.00	12	\$24,000.00	
- Landscaping	LUMP	\$7,500.00		\$7,500.00	
C. Bornstein's					
- Stalls	EA	\$1,500.00	24	\$36,000.00	
- Lighting Standards	EA	\$2,000.00	4	\$8,000.00	
- Landscaping	LUMP	\$3,800.00		\$3,800.00	
D. 15th-17th Street					
- Stalls	EA	\$3,000.00	52	\$156,000.00	
- Landscaping	LUMP			\$9,500.00	
10. Astor/10th Street Redevelopment					
A. Street Resurfacing	LF	\$300.00	680	\$204,000.00	
B. Sidewalk Resurfacing	SF	\$8.50	13,000	\$110,500.00	
C. Lighting Standard	EA	\$2,000.00	12	\$24,000.00	
D. Street Furniture	LUMP			\$14,000.00	
E. Street Trees	EA	\$1,000.00	15	\$15,000.00	
11. Streetscapes					
A. Crosswalks (Arterial Pedestrian Crossing)	SF	\$8.50	77,300	\$657,050.00	
B. 8th Street					
- Sidewalk Resurfacing	SF	\$8.50	10,000	\$85,000.00	
- Light Standards	EA	\$2,000.00	22	\$44,000.00	
- Street Trees	EA	\$1,000.00	22	\$22,000.00	
- Street Furniture	LUMP			\$15,000.00	

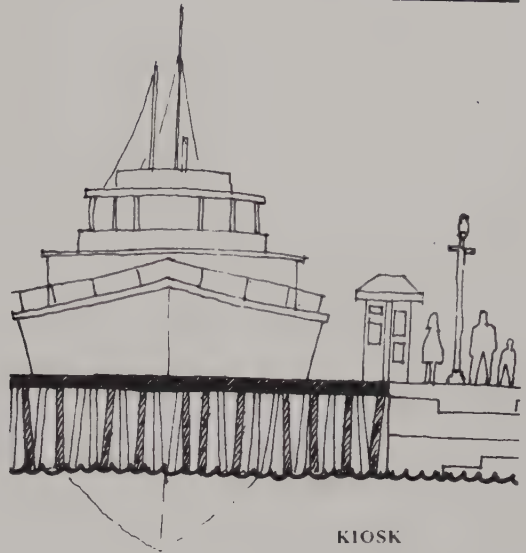
Cost Analysis

CONSTRUCTION ITEM	CONST UNIT	UNIT COST	# OF UNITS	TOTAL COST	SUMMATION
<hr/>					
C. 10th Street					
- Sidewalk Resurfacing	SF	\$8.50	8,000	\$68,000.00	
D. 12th Street					
- Sidewalk Resurfacing	SF	\$8.50	5,200	\$44,200.00	
- Light Standard	EA	\$2,000.00	12	\$24,000.00	
- Street Trees	EA	\$1,000.00	12	\$12,000.00	
- Street Furniture	LUMP			\$12,000.00	
E. 14th Street					
- Sidewalk Resurfacing	SF	\$8.50	4,000	\$34,000.00	
- Light Standard	EA	\$2,000.00	8	\$16,000.00	
- Street Trees	EA	\$1,000.00	8	\$8,000.00	
- Street Furniture	LUMP			\$10,000.00	
F. 17th Street to Heritage Museum					
- Sidewalk Resurfacing	SF	\$8.50	9,000	\$76,500.00	
- Light Standard	EA	\$2,000.00	20	\$40,000.00	
- Street Trees	EA	\$1,000.00	20	\$20,000.00	
- Street Furniture	LUMP			\$13,000.00	
G. 14th - 15th Street					
- Sidewalk Resurfacing	SF	\$8.50	3,500	\$29,750.00	
- Light Standard	EA	\$2,000.00	6	\$12,000.00	
- Street Trees	EA	\$1,000.00	6	\$6,000.00	
12. Landscaping					
- People Place Park	LUMP			\$12,000.00	
- Eastern Gateway	LUMP			\$20,000.00	
- Western Gateway	LUMP			\$10,000.00	
13. Signalization	LUMP			\$100,000.00	
17th & Marine Drive					
	SUB-TOTAL:	PUBLIC		\$1,290,500.00	
		PRIVATE/PUBLIC		\$2,294,500.00	
	GRAND TOTAL:			\$3,585,000.00	

Cost Analysis

ASTORIA WATERFRONT - PLAN B
PRIVATELY FUNDED PROJECTS

CONSTRUCTION ITEM	CONST UNIT	UNIT COST	# OF UNITS	TOTAL COST	SUMMATION
A. COLUMBIA RIVER MARITIME MUSEUM					
1. Pedestrian Paths					
A. Pathway (10' Wide Concrete)	SF		\$2.50	9,800	\$24,500.00
B. Lightling	EA		\$2,000.00	15	\$30,000.00
C. Street Furniture	LUMP				\$18,000.00
2. Special Pavement Area					
A. Main Entrance Area (Brick Concrete Base)]	SF		\$8.50	31,775	\$270,087.50
3. Marinas					
A. Museum Marina (50 Slips/Dock)	EA		\$10,000.00	50	\$500,000.00
Floating Breadwater	SF		\$150.00	680	\$102,000.00
4. Train Station (Redvelopment of Former Train Station	SF		\$50.00	5,175	\$258,750.00
5. Parking					
- Stalls	EA		\$1,500.00	200	\$300,000.00
- Light Standards	EA		\$2,000.00	60	\$120,000.00
- Paths (6' Wide Concrete)	SF		\$2.50	3,000	\$7,500.00
6. Maritime Docks					
A. New Maritime Dock	SF		\$40.00	4,500	\$180,000.00
7. West Plaza					
A. Tower	To Be Determined				
B. Interpretive Ship	To Be Determined				
C. Lighting Standard	EA		\$2,000.00	6	\$12,000.00
D. Kiosks	EA		\$2,000.00	6	\$12,000.00
8. East Plaza					
A. Paving	SF		\$3.00	12,500	\$37,500.00
B. Amphitheatre	SF		\$20.00	5,000	\$100,000.00
C. Lighting Standard	EA		\$2,000.00	8	\$16,000.00
D. Kiosks	EA		\$2,000.00	4	\$8,000.00
9. Picnic Area (Tables, Concrete, Pad)	EA		\$1,000.00	10	\$10,000.00
11. Landscaping	LUMP				\$46,000.00
12. Restroom	SF		\$175.00	300	\$52,500.00
				SUB-TOTAL:	\$2,104,837.50
B. VIEW DECKS					
1. 6th Street Dining Decks	SF		\$40.00	1,500	\$60,000.00
C. COMMERCIAL DEVELOPMENT					
1. 6th Street Commercial Building	SF		\$60.00	750	\$45,000.00
				GRAND TOTAL:	\$2,209,837.50
					=====



APPENDIX

ASTORIA CIVIC PIER

WATERFRONT PROPERTY AND BUSINESS OWNERS QUESTIONNAIRE

Who answered the questionnaire:

- Bornstein Seafood (Douglas Heater)
- Corinne Ricciardi
- Pacific Power and Light Co. (Dan Hitchcock, Timber Stevens, Jim Welch, Jim McClain)
- Barbey Investment Company Building (Graham Barbey)
- Far West Data Control (Dave Roberts)
- Blair Henningsgaard
- Terry Hahn
- Stan Hauer
- Don Haskell
- Carol Haskell
- Ocean Foods at Astoria (Grant Larson)
- Persona Vintage Clothing (Rosetta Hurley)
- Charles Farmer
- Astoria Data Service (Martin Adams)
- Astoria Bargain Center (Gertrude Maki)
- Arthur _____

What type of businesses:

- Hardware, Marine Engines, Household Merchandise, Classic Cars & Parts
- Computer Service
- Radio Broadcasting
- Vintage Clothing Costume Rentals
- Seafood Processing (2)
- Employment Counseling
- Property Management
- Retail & Service of Bicycles, Lawnmowers/Locksmith
- Automotive, Marine & Industrial Parts
- Law Office
- Software Sales
- Dentists' Offices (3)
- Electric Service
- Art Gallery/Espresso Bar

Hours of operation:

5:00 a.m. - midnight	1
6:00 a.m. - 5:00 p.m.	1
8:00 a.m. - 4:00 p.m.	1
8:00 a.m. - 5:00 p.m.	3
8:00 a.m. - 6:00 p.m.	5
9:00 a.m. - 5:00 p.m.	6
9:00 a.m. - 5:30 p.m.	7
11:00 a.m. - 5:00 p.m.	1
Weekends	3
24 Hours	1
Closed	1

Number of seasonal businesses: 3

Summer and Christmas, July and October, April and October

Number of year-round businesses: 14

Rail	0
Truck	10
Van	4
Car	10
Boat	2

At what hours:

Varies	3
8:00 a.m. - 4:00 p.m.	1
9:00 a.m. - 5:00 p.m.	5
Before 11:00 a.m.	1
Mornings	2
Late Evenings	1
24 hrs./day, 7 days a week	1

How do goods leave businesses:

Rail	0
Truck	9
Van	6
Car	9
Boat	1

By Foot	1
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At what hours:

8:00 a.m. - 4:00 p.m.	1
8:00 a.m. - 6:00 p.m.	1
9:00 a.m. - 5:00 p.m.	5
Before 11:00 a.m./After 5:00 p.m.	1
Afternoon	1
Varies	3
24 hrs./day, 7 days a week	1

Does this property or business depend upon or interact with other properties on or near the waterfront?

Yes	10
No	5

How?

Share Parking	1
Foot Traffic	3
Service to Local Businesses	3
Purchasing Supplies/Services from Other Businesses	1
Common Sewer Interceptor Line	1

Where are these other properties/businesses?

Nearby	4
Knappton to Englund Marine	1
9th - 10th Streets	2
On Waterfront or within a Block of It	1
14th Street	1

What are the 5 major advantages in doing business on the waterfront?

Tourist Attraction, Customer Contact, Foot Traffic	8
View	6
Near Downtown Location	3
Near other Suppliers/Businesses	3

Accessibility	3
Water Access	2
Ease of Parking	1
None	3
Proximity to Restaurants	1
Visibility	1
Potential for Growth	1
✓ Electrical Ground System in River	1

What are the ways the present waterfront area supports this property?

✓ Tourist Oriented/Foot Traffic	5
✓ Year-round Support of Businesses	4
✓ Location	3
Vehicular Access	1
Marine Oriented/Fishermen	1
City of Astoria's Cooperation	1
View	1

What are the 5 major difficulties in doing business on the waterfront?

Parking	11
Rentals Expensive	11
Congestion/Access	6
Expensive to Maintain Piling	4
Access by Trucks on Street Ends - 7th & 8th	2
Marine Drive One-Way/Marine Drive Traffic	2
Traffic along Railroad	1
Trucks Unloading Causes Congestion	1
Dead End Street	1
Access for Fire Fighting from Land Side	1
No Development of Traffic Access along Railroad Tracks	1
Upkeep of Railroad Property	1
Scouring Effects of Deep Water Channel Currents	1
Docks	1
Lack of Boat Mooring (temporary)	1
Building & Structure Maintenance Costs	1
State Land Board Assessments Too High	1
Limited Space to Expand	1
Not Enough Foot Traffic	1

Not a Cohesive Retail Area	1
Present Regulations Preclude Line Extension North of Railroad Tracks	1
Conflicts with Other Businesses over Property Use	1
Noise from Fish Plants	1
Smell from Fish Plants	1

What are the ways the present waterfront area makes the use of this property or business difficult?

- Parking on railroad. 1
- Make more accessible. 1
- For a seafood processor trying to convert the waterfront into a public access area is a conflict of interest. 1
- Public parking in what is supposed to be reserved tenant parking. 2
- Increased traffic during tourist season. 2
- Access difficult due to chairwall construction. 1
- Voltage limitations. 1
- Parking on 7th, 8th and 9th. 1
- Fred Meyer is going to kill Astoria's downtown business. 1

What could the waterfront plan do to support and strengthen this property or business?

More Parking	4
Make more attractive.	2
Landscaping	2
More Accessible	1
(Keep access clear for our land)	
Fire Protection	1
Truck Loading Access	1
Remodel buildings to have view of waterfront.	1
Maintain wood street dock ends.	1
Public should be involved.	1
Plan <u>around</u> existing businesses.	1

What could the waterfront plan do to keep from detracting from this property of business?

- Clean up and make safer from both land and water. 1
- Keep property owners from letting their property look run down. 1
- Limit tall buildings on riverfront. Avoid carnival/boardwalk atmosphere. 1
- Don't attract too many people. 1
- Don't plan to send public into potentially dangerous dock areas. We need

- to preserve the few successful businesses Astoria has. 1
- Maintain privacy of business. 2
- Support private enterprise - do not hinder by rules. 1
- Preserve our current and past fishing industry. 1

What are your future plans for this property or business?

Expand	5
Upgrade Exterior/Structure	2
Small Expansion	1
Success	1
Maintenance of building, dock and parking.	1
Profit from location and activities.	1
Possible new building.	1
Advertise outside of area to expand clientele.	1
Remain at present location, unless things get worse.	1

What waterfront characteristics are unique to Astoria?

Ships	5
Commercial Waterfront (working atmosphere, working people)	5
River Traffic	4
Historic Fishing Area	4
Old Waterfront Buildings	1
Railroad	1
Shipping of Lumber	1
Constant Change of Scenery	1
Proximity to Downtown	1
Not Crowded	1

How might these unique characteristics be included in the waterfront plan?

- City should accept liability for tourists near waterfront. 1
- Expansion around historic background. 1
- Incorporate public viewing areas and boardwalks with working waterfront. 1
- Picnic areas and restaurants. 1
- Floating docks. 1
- Provide viewing areas that do not conflict with working areas. 1
- Visual cannery operations were always a tourist draw. 1
- Exhibits 2
- Public access to view working areas. 2

- Clean up waterfront near railroad tracks. 1
- Access to artists. 1

What waterfront activities might be most interesting to the public?

Shopping	2
Eating	2
Working Boats	4
Public Walkways	1
Access to Docks	1
Dock Fishing Area	3
Viewing Areas	1
Explanation of Shipping Activities	2
Historical Points	2
Fishing	2
Fish Processing	1

Is this business such that the public could view it or interact with it in some manner?

Yes	4
No	11
Maybe	1

If yes, how?

Consumer-oriented	3
Water Tours	1

What things would you like the waterfront plan to provide for the community?

Attraction of Tourists	2
Public Kept in Specified Areas	1
Parking	2
Link between Commercial & Waterfront	2
Area for Socializing	1
Area that Provides Jobs	1
Pride in Area (attractive waterfront)	2
Use of Area for Recreation, Relaxation	1
Clean up	1
Access to River	1
Public Walkways	2
Access to Docks	1
Planters	1

Lighting as on 10th Street	1
Move or Fix West End Basin	1
A Plan for the Future	1
Marine-oriented Gift Shop	1
Moorage for Vessels for the Public	1

What things would you not like the waterfront plant to provide for the community?

Don't develop retail to compete with downtown.	1
Restrictive building codes, zoning.	2
Attracting tourists without expanding parking.	1
A "Coney Island" type of tourist trap.	2
No camping, transient loitering.	2
Interference to operations of existing business.	1
Don't keep changing the plan.	1
Anything unsafe.	1

What is your vision of the waterfront and how will it evolve?

- Community supportive of seafood and lumber industries.
- Access to prevent crowding (one of Astoria's greatest assets).
- Accessibility.
- Selective, aesthetically pleasing developments.
- A place where people enjoy going, where they can learn a bit about Astoria.
- Connection from Red Lion to the old railroad station.
- Fish processing, plants, boat moorage.
- Visitor access.
- Linear park winding around the present commercial waterfront businesses with vistas of the river.
- Direct connections to commercial strip important.

Other Comments:

- Commercial Street and Marine Drive to get a bypass from John Day Bridge to Young's Bay Bridge to get through trucks off 2 downtown streets.
- Suggest a marina-type development that emphasizes sport and pleasure boats, restaurants, shops, docks.
- Review past development plans.
- Public/private developments such as of 6th Street are very positive and exciting.
- What effect will possible improvement of Burlington Northern have on

Astoria civic plan?

- Big stadium-like structure and the regatta was mostly viewed from speed boats, fish boat races, lighted vessels at night before it burned.

PRIORITY LIST

At the last public meeting in December of 1989, the following questionnaire was distributed to everyone who attended the meeting:

ASTORIA WATERFRONT STUDY

Dear Concerned Citizen:

The improvements proposed by the Waterfront Master Plan will be a wonderful asset to the community when completed. Depending on the cost of these improvements, it is possible that the City will choose to implement the proposed improvements one at a time, on a project by project basis. We need your assistance in establishing a priority list for development of the waterfront. Please review the list of improvements in each alternative plan below and identify the order in which you would like to see them constructed.

Proposed Improvements for Alternative Plan A & B

- A. Pedestrian Pathways
- B. Special Pavement Area
- C. Trolley System
- D. View Area Development at Street Ends
- E. Deck Area Built in Association with Existing Buildings
- F. Knappton Building Redevelopment
- G. Commercial/Retail Complex at 10th Street
- H. Restrooms
- I. Improved Parking

- J. Astor/10th Street Resurfacing
- K. Columbia River Maritime Museum Improvements
- L. Streetscapes
- M. Landscaping

Priority List

List the letters from the proposed improvement list above in order of priority with Number 1 being the highest priority.

- 1. _____
- 2. _____
- 3. _____
- 4. _____
- 5. _____
- 6. _____
- 7. _____
- 8. _____
- 9. _____
- 10. _____
- 11. _____
- 12. _____
- 13. _____

RESULTS

The following is summation of the 76 questionnaires that were returned:

Number #1 Priority

- (31) A
- (0) B
- (2) C
- (12) D
- (6) E
- (4) F

- (1) G
- (0) H
- (8) I
- (0) J
- (6) K
- (2) L
- (6) M

Pedestrian pathways, view area development at street ends and improved parking received the most votes as priority #1.

Number #2 Priority

- (12) A
- (1) B
- (6) C
- (10) D
- (7) E
- (5) F
- (4) G
- (5) H
- (5) I
- (1) J
- (3) K
- (4) L
- (4) M

Pedestrian pathways, view area development at street ends deck areas built in association with existing buildings received the most votes as priority #2

Number #3 Priority

- (6) A
- (1) B
- (2) C
- (12) D
- (9) E
- (5) F
- (8) G
- (6) H

- (8) I
- (2) J
- (4) K
- (4) L
- (2) M

View area development at street ends, Deck areas built in association with existing buildings, commercial/retail complex at 10th Street and Improved parking received the most votes for priority #3.

Priority #4

- (3) A
- (5) B
- (8) C
- (6) D
- (14) E
- (1) F
- (4) G
- (8) H
- (9) I
- (0) J
- (5) K
- (4) L
- (4) M

Deck areas built in association with existing buildings, improved parking and restrooms received the most votes for priority #4.

Priority #5

- (2) A
- (4) B
- (5) C
- (6) D
- (9) E
- (4) F
- (4) G
- (12) H
- (5) I

- (2) J
- (3) K
- (6) L
- (7) M

Restrooms, deck areas built in association with existing buildings, and landscaping received the most votes for priority #5.

Priority #6

- (4) A
- (3) B
- (4) C
- (6) D
- (7) E
- (4) F
- (3) G
- (5) H
- (9) I
- (2) J
- (4) K
- (11) L
- (5) M

Streetscapes, improved parking and deck areas built in association with existing buildings received the most votes for priority #6.

Priority #7

- (4) A
- (3) B
- (6) C
- (6) D
- (2) E
- (9) F
- (1) G
- (2) H
- (5) I
- (3) J
- (6) K
- (7) L

- (8) M

Knappton Building redevelopment, landscaping, and streetscapes received the most votes for priority #7.

Priority #8

- (4) A
- (7) B
- (2) C
- (4) D
- (2) E
- (3) F
- (4) G
- (5) H
- (8) I
- (5) J
- (3) K
- (9) L
- (7) M

Streetscapes, improved parking, special pavement areas and landscaping received the most votes for priority #8.

Priority #9

- (1) A
- (8) B
- (3) C
- (1) D
- (4) E
- (3) F
- (6) G
- (11) H
- (2) I
- (4) J
- (6) K
- (6) L
- (7) M

Restrooms, special pavement areas and landscaping received the most votes for priority #9.

Priority #10

- (3) A
- (6) B
- (2) C
- (1) D
- (1) E
- (8) F
- (4) G
- (6) H
- (2) I
- (12) J
- (5) K
- (5) L
- (4) M

Astor Street/10th Street Resurfacing, Knappton Building redevelopment, special pavement areas and restrooms received the most votes for priority #10.

Priority #11

- (0) A
- (6) B
- (1) C
- (3) D
- (2) E
- (6) F
- (7) G
- (4) H
- (6) I
- (6) J
- (8) K
- (7) L
- (6) M

Maritime Museum improvements, streetscapes, commercial/retail improvements at 10th Street, special pavement areas, improved parking, Astor/10th Street improvements, trolley system and landscaping received the most votes for priority #11.

Priority #12

- (0) A
- (9) B
- (10) C
- (10) D
- (3) E
- (4) F
- (5) G
- (3) H
- (0) I
- (15) J
- (2) K
- (1) L
- (5) M

Astor/ 10th Street resurfacing, trolley system and special pavement areas received the most votes for priority #12.

Priority #13

- (1) A
- (7) B
- (16) C
- (0) D
- (0) E
- (2) F
- (12) G
- (2) H
- (1) I
- (6) J
- (7) K
- (2) L
- (1) M

To Mayor Bruce Jones and Astoria City Council members:
Joan Herman, Roger Rocka, Tom Bronson and Jessayign Gracewest

Columbia Riverfront

The main navigational channel of the Columbia River sweeps past Astoria's downtown riverfront, closer to the shoreline than in any other part of the river. Every walker or bike rider or jogger along Astoria's Riverwalk enjoys the open sky, magnificent views and maritime activity on the Columbia River.

And the River, in turn, offers views of the shore.

At least since 1986, the city had considered revitalizing its Columbia Riverfront. When Jim Flint was city manager his first project was the Sixth Street River Park, a viewing platform. My story and photo of the 6th street platform appeared in OREGON COAST magazine.

Lack of safety down on the riverbank, prompted Mayor Edith Hennnigs-gaard to invite Murase Associates of Portland for advice. **Murase's Astoria Waterfront Planning Statement**, June 1990, was the initial articulation of a "vision." Citizen advisors, technical advisors, and affected property owners contributed to the statement. They found "no distinct circulation system along the

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In effect, **Murase** advised the Mayor,

"Tell your city council to clean up the waterfront."

So, City of Astoria sponsored Saturday morning work parties. Volunteer townspeople by the dozens with lopers and spades in hand, each cleared up a section of the river's edge of old tires and car parts, cans and bottles, bedsprings, old metal, tide debris, old clothing, just plain junk. Willis Van Dusen was there among others with pickup trucks to haul stuff to the dump. Rosemary Johnson was there to give out packets of wild flower seeds to scatter.

Each Saturday the Lions club was there to offer lunch.

Key to implementing the "vision" was Burlington Northern Railroad's abandonment of the track from Smith Point to Tongue Point. The city immediately filed a notice of Interim Trail Use under the National Trails Act which allowed the city to become Steward of the Corridor.

And the city has been a good steward. For the next 20 years the city Community Development dept. Directed by Brett Estes proceeded -- increment grant by increment grant --to revitalize the Waterfront Riverwalk. It became a SIGNATURE feature of Astoria, promoted by the Chamber of Commerce, the cruise ships and media outlets. In more recent years, nighttime lighting posts added to the pleasure of the ^{10.}four-mile Riverwalk.

The **Murase Plan** had also advised the city "to establish a special design district ordinance, complete with appropriate guidelines to manage the function and design of specific blocks within the downtown waterfront area to ensure this vision becomes a reality." The ordinance became the Riverfront Vision Plan.

By 2007, however, several large projects were underway north of the 16th to 41st street riverfront. Which prompted the city to hold community open houses that winter at the Red Building, the Maritime Museum and the Astoria Middle School. All were well attended, with much documentation of intended uses voiced by townspeople, with comments reinforced by a community survey. Subsequently, a four-phase Riverfront Vision Plan was adopted by the Astoria City Council in 2009.

In more recent years both west and east reaches of the riverfront have been developed privately near the west mooring basin and beyond the east mooring basin at 39th st.

Concurrently, three segments of the Riverfront Vision Plan thrive along three stretches of the of the Columbia waterfront with land-use guidelines in place. The final section, the Urban Core between Second and 16th streets has become the longtime task of the City Planning Commission.

As of today, the Planning Commission is considering height and width of buildings in the Bridge Vista area of the waterfront, consistent with current city building codes. And the need to address rights of private property owners in its waterfront area.

A **public hearing** will be held Monday, June 17, at 7 p.m. by the Astoria City Council in the council chambers at City Hall to consider a requested amendment to the City Development Code. The amendment will address code requirements in the Civic Greenway and the Neighborhood Greenway as well as the Bridge Vista area. And will expand responsibilities of the Design Review committee and how affected by various sections of the code. Visitor lodging will also be considered in amendments to the code.

Today, from Millpond village looking west the eye may travel the curve of the shore to the Sixth st. Viewing platform, under the bridge all the way to the mouth of the river. And north to the Washington peninsula. This is the remaining panoramic view of the Columbia River from the Riverwalk. Why would any city official -- elected or appointed or hired -- destroy this view?

We've recognized our local history with a riverscape that defines our community. Let's not trivialize our history already made in exchange for a history we may not be proud of.

Juanita Price

The Retirement Village
947 Olney ave, Apt 309
Astoria, OR 97103

